

# BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT TAXIWAY A PAVEMENT REHABILITATION

BIRMINGHAM AIRPORT AUTHORITY (BAA)  
5900 MESSER AIRPORT HWY  
BIRMINGHAM, AL 35212

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SARCOR, LLC  
215 19TH STREET SOUTH  
BIRMINGHAM, AL 35203

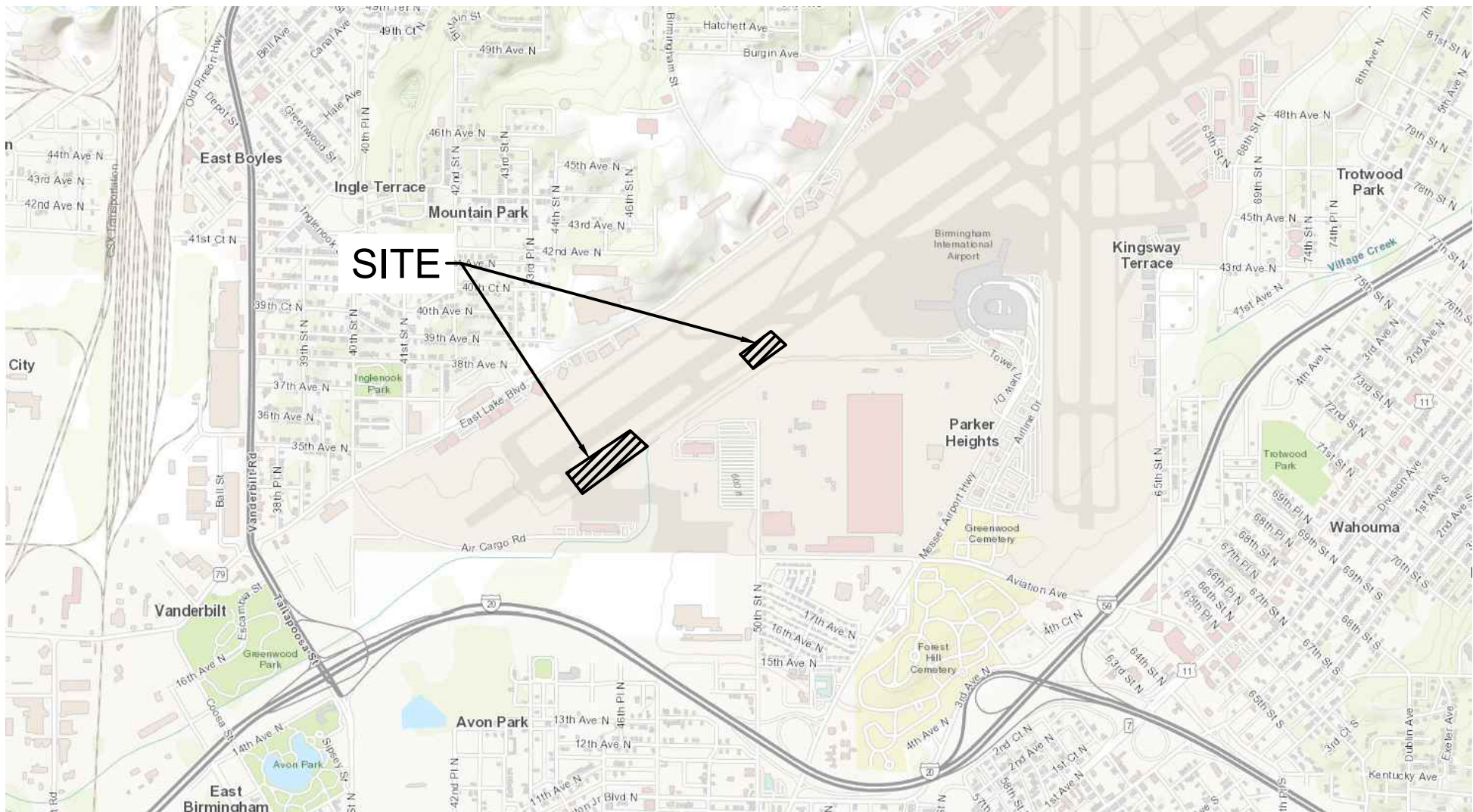
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EMAIL: JEFF@SARCORLLC.COM

## GENERAL NOTES

1. ALL PHASES OF SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE OWNER/DEVELOPER SPECIFICATIONS. THE ENGINEER HAS MADE EVERY EFFORT TO SET FORTH IN THE CONSTRUCTION AND CONTRACT DOCUMENTS THE COMPLETE SCOPE OF WORK. THE CONTRACTOR BIDDING THE JOB IS NEVERTHELESS CAUTIONED THAT MINOR OMISSIONS IN THE DRAWINGS AND/OR SPECIFICATIONS SHALL NOT EXCUSE THE CONTRACTOR FROM COMPLETING THE PROJECT AND IMPROVEMENTS IN ACCORDANCE WITH THE INTENT OF THESE DOCUMENTS. ALL WORK SHALL BE AS INDICATED AND STIPULATED ON THE DRAWINGS AND IN THE SPECIFICATIONS.
2. THE CONTRACTOR SHALL VISIT THE JOB SITE PRIOR TO THE SUBMISSION OF BIDS TO FAMILIARIZE HIM/HERSELF WITH THE FIELD CONDITIONS AND TO VERIFY THAT THE PROJECT CAN BE CONSTRUCTED IN ACCORDANCE WITH THE INTENT OF THESE DOCUMENTS.
3. THE CONTRACTOR SHALL OBTAIN AUTHORIZATION TO PROCEED WITH CONSTRUCTION PRIOR TO STARTING WORK ON ANY ITEM NOT CLEARLY DEFINED BY THE CONSTRUCTION DRAWINGS AND/OR CONTRACTOR DOCUMENTS.
4. THE CONTRACTOR SHALL SUPERVISE AND DIRECT THE PROJECT DESCRIBED HEREIN. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ALL CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES, AND FOR COORDINATING ALL PORTIONS OF WORK UNDER THE CONTRACT. EACH CONTRACTOR SHALL COOPERATE WITH THE OWNER'S REPRESENTATIVE, AND COORDINATE HIS WORK WITH THE WORK OF OTHERS.
5. THESE DRAWINGS ARE FORMATTED FOR 22" X 34". OTHER SIZE VERSIONS ARE NOT PRINTED TO THE SCALE CALLED OUT OR SHOWN.
6. CONTRACTOR IS RESPONSIBLE FOR THE COST AND FEES ASSOCIATED WITH THE EXECUTION OF WORK. THIS INCLUDES ANY AND ALL PERMITS AND IMPACT FEES INCLUDING, BUT NOT LIMITED TO, DEMOLITION PERMITS, ADEM PERMIT FEES, BUILDING PERMITS, SANITARY SEWER FEES AND IMPACT FEES, WATER TAP FEES, ETC.
7. A. IN THE CASE OF UNFORESEEN CONSTRUCTION COMPLICATIONS OR DISCREPANCIES, THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE ENGINEER OF RECORD IN WRITING.  
  
B. IF THE CONTRACTOR DAMAGES ANY (EXISTING) SITE FEATURES DURING CONSTRUCTION, HE SHALL AT HIS OWN EXPENSE REPLACE OR REPAIR THE FEATURES IMMEDIATELY TO ORIGINAL CONDITION AND QUALITY AS APPROVED BY THE OWNER OR DESIGNATED REPRESENTATIVE.  
  
C. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL SAFETY CODES OF THE GOVERNING MUNICIPALITIES.  
  
D. DEVIATIONS FROM THESE PLANS AND ANY ASSOCIATED SPECIFICATIONS WITHOUT PRIOR WRITTEN CONSENT OF THE ENGINEER OF RECORD MAY CAUSE WORK TO BE UNACCEPTABLE.  
  
E. WHEN APPLICABLE, FIRE DEPARTMENT ACCESS SHALL BE ALWAYS MAINTAINED.  
  
F. WHEN APPLICABLE, SUFFICIENT BARRICADES, LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES AND METHODS WHICH MAY BE NECESSARY FOR THE PUBLIC SAFETY AND PROTECTION SHALL BE IN ACCORDANCE WITH GOVERNING ORDINANCES AND M.U.T.C.D. (CURRENT EDITION) AND SHALL BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION.

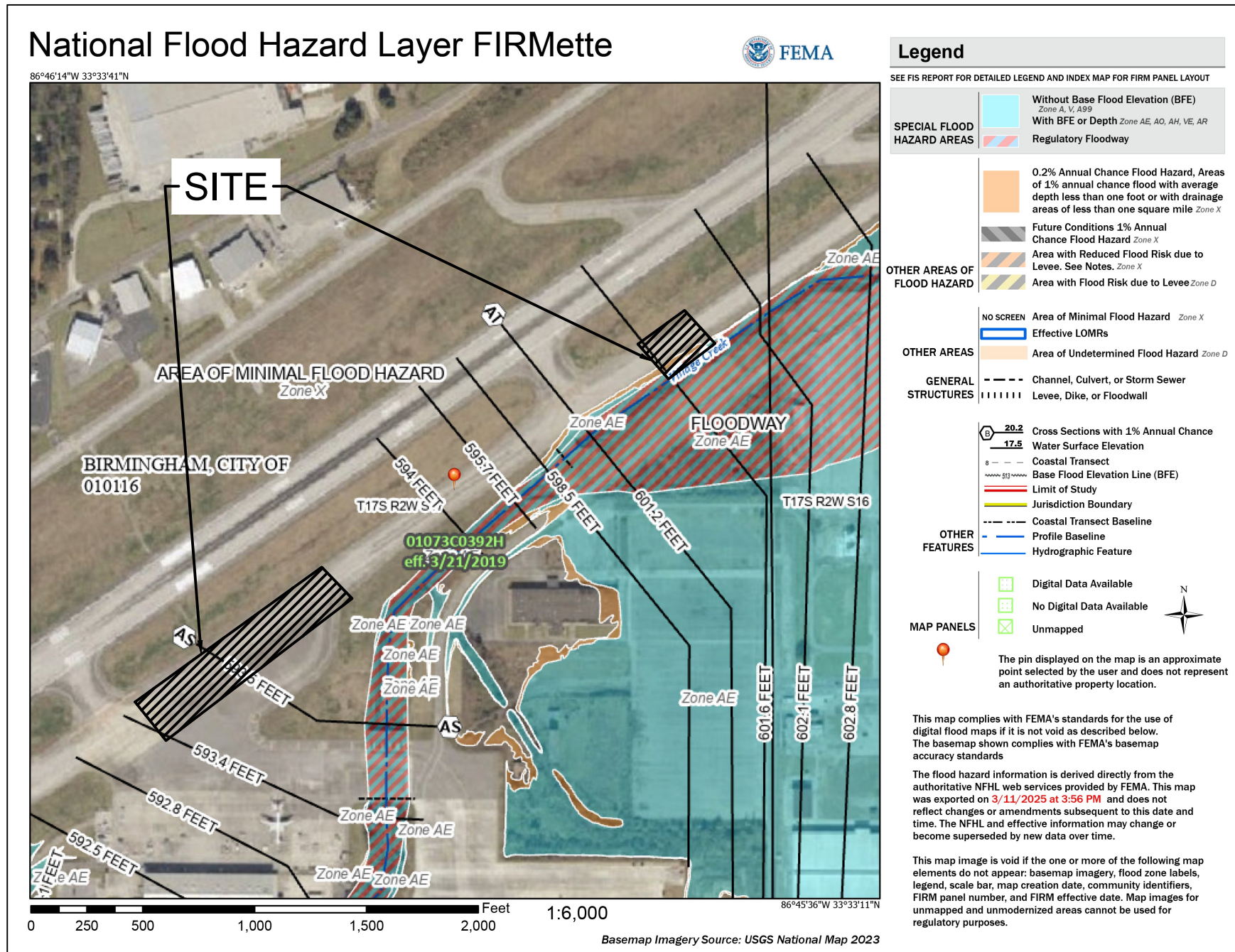
## SHEET INDEX

C-1.0 TITLE SHEET  
C-2.0 PROJECT NOTES  
C-3.0 PHASING PLAN  
C-4.0 REHABILITATION PLAN  
C-4.1 REHABILITATION PLAN  
C-5.0 MARKING PLAN  
C-6.0 TYPICAL SECTION & DETAIL SHEET



## VICINITY MAP

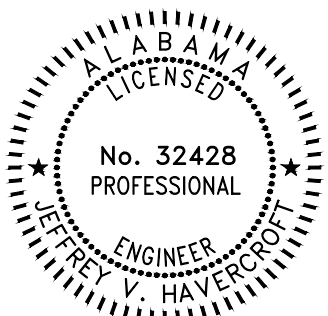
N.T.S  
PARCEL IDENTIFICATION: 23 00 17 1 000 006.000 &  
23 00 17 4 000 001.000



# FEMA MAP

N.T.S  
EFFECTIVE DATE 03/21/2019  
SITE IS LOCATED WITHIN FLOOD ZONE X

THERE IS NO FLOODPLAIN ON THIS PROPERTY AS PER FIRM PANEL 01073C0392H DATED 03/21/2019. NO WORK IS BEING DONE WITHIN A FLOODPLAIN. THE PROPERTY SHOWN HEREON LIES WITHIN ZONE X AND IS NOT WITHIN A SPECIAL FLOOD HAZARD AREA.



DESIGNED BY:	PROJECT NUMBER:	MARK	DESCRIPTION	DATE
JH	25-E-01-03100		NOT FOR CONSTRUCTION	---
			ISSUED FOR REVIEW / COMMENT	3/27/2025
DJB	ISSUE DATE: --/--/2025		FINAL DESIGN	4/10/2025
SCALE:	N.T.S.			

**SARCOR, LLC**

215 19 ST N, SUITE 101  
BIRMINGHAM, AL 35203

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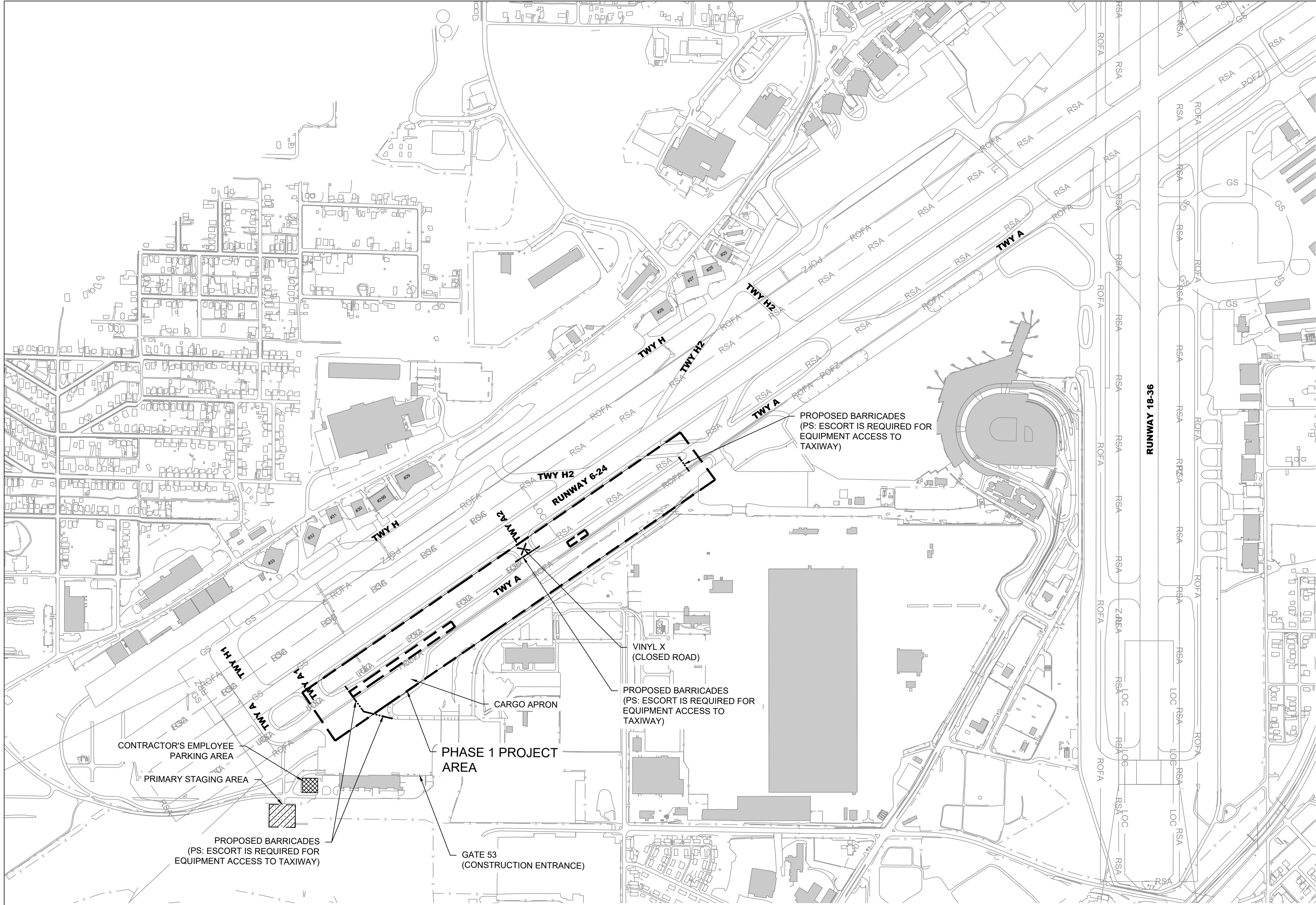
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C-1.0  
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## CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS

SCHEDULE OF QUANTITIES			
ITEM NO.	DESCRIPTION	APPROX. QUANTITY	UNIT
C-102.4	INSTALL CONSTRUCTION SITE SAFETY MEASURES	1	LSUM
C-102.3	REMOVE CONSTRUCTION SITE SAFETY MEASURES	1	LSUM
C-105	ESTABLISH SITE ACCESS AND MATERIAL STAGING AREAS	1	LSUM
P-101	REMOVAL OF EXISTING PAVEMENT	2,728	SY
P-401	PLACEMENT OF BINDER ASPHALT LAYER 5" THICK	818	TON
P-403	PLACEMENT OF SURFACE WEARING LAYER 3" THICK	491	TON
P-620	PAVEMENT MARKING INCL. PRESERVATION OF CENTERLINE	1	LSUM





AOA GATE GUARD RESPONSIBILITIES

1. AOA GATE ACCESS PROCEDURE (EACH TIME ANY VEHICLE ENTERS)
- KEEP THE GATE CLOSED WHILE IT IS NOT BEING USED.
  - MAKE POSITIVE CONTACT WITH THE DRIVER AND ANY PASSENGERS.
  - INSPECT I.D.'S, ENSURE THEY ARE VALID, AND CHECK ANY AIRPORT ISSUED BADGES AGAINST THE STOP LIST BEFORE ENTRY IS PERMITTED.
  - VISUALLY INSPECT ALL STORAGE AREAS INSIDE AND OUTSIDE OF THE VEHICLE. ENSURE THERE ARE NO PROHIBITED ITEMS PRESENT INCLUDING FIREARMS - VERBALLY INQUIRE.
  - IF THE DRIVER DOES NOT POSSESS AN AIRPORT BADGE, DO NOT PERMIT AOA ENTRY UNTIL AN AIRPORT BADGED PERSON WITH ESCORT PRIVILEGES HAS CONFIRMED THEY ARE TAKING OVER THE ESCORT OF THE NON-BADGED PERSON.

2. AOA GATE GUARD GUIDELINES

- NO PERSONAL VEHICLES ARE PERMITTED TO ENTER THE AOA.
  - ALL VEHICLES ENTERING THE AOA MUST DISPLAY APPROPRIATE COMPANY SIGNAGE ON BOTH SIDES OF THE VEHICLE.
  - GUARDS MUST ENSURE ALL NON-BADGED PERSONNEL ENTERING THE AOA POSSESS VALID I.D., EXPIRED I.D. IS INVALID.
  - NON-BADGED PERSONNEL MUST ALWAYS BE ESCORTED BY BADGED PERSONNEL WITH ESCORT PRIVILEGES.
  - GUARDS MUST POSSESS THE MOST RECENTLY ISSUED STOP LIST.
  - GUARDS MAY NEVER LEAVE AN UNLOCKED GATE UNATTENDED.
  - ALL PERSONNEL ENTERING THROUGH THE GATE MUST BE CHECKED. THIS INCLUDES AIRPORT, TSA, FAA, AND CONTRACTOR/SUBCONTRACTOR PERSONNEL.
  - GATE GUARDS MUST BE AWARE THEY ARE SUBJECT TO THE SAME SECURITY POSTURE TESTING AS THE REST OF THE AIRPORT.

LEGEND:

- PROJECT AREA
- CONTRACTOR STAGING AREA
- CONTRACTOR EMPLOYEE PARKING AREA
- #-TSA

EXISTING TAXIWAY SAFETY AREA  
(ADG AS INDICATED BY #)
- #-TOFA

EXISTING TAXIWAY OBJECT FREE AREA  
(ADG AS INDICATED BY #)
- ROFA —

RUNWAY OBJECT FREE AREA
- RSA —

RUNWAY SAFETY AREA
- GS —

GLIDE SLOPE CRITICAL AREA
- X —

EXISTING FENCE

SECURITY

1. GENERAL INTENT: IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RPR AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SAFETY/SECURITY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT.

2. CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR AND SUB-CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.

3. ACCESS TO THE SITE: CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL TRAFFIC TO/FROM PROJECT CONSTRUCTION AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT AT ALL TIMES ONCE INSIDE AOA SECURITY FENCE. A CONTRACTOR GATE GUARD SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT ANY AOA

ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE AOA SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE BADGED AND FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNAGE FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS.

4. MATERIALS DELIVERY TO THE SITE: ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE NAME "BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INAPPROPRIATELY. DELIVERIES MAY BE ESCORTED ON SITE BY A PROPERLY BADGED EMPLOYEE.

5. CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA, AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING, AND/OR WARNING DEVICES VISIBLE FOR DAY/NIGHT USE AT THE LOCATIONS SHOWN IN THE PLANS. TEMPORARY BARRICADES, FLAGGING, AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT OPERATIONS.

6. SECURITY IDENTIFICATION - PERSONNEL: PRIOR TO BEGINNING WORK, ALL CONTRACTOR OR SUBCONTRACTOR EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. WHO WILL REQUIRE ACCESS TO, BE OPERATING VEHICLES OR EQUIPMENT ON, OR BE WORKING UNESCORTED ON THE AIRFIELD WILL BE REQUIRED TO OBTAIN AN AIRPORT ISSUED SECURITY IDENTIFICATION BADGE, IN ACCORDANCE WITH THE AIRPORT OPERATIONS SECURITY PROGRAM.

ALL PERSONNEL MUST DISPLAY AIRPORT ISSUED IDENTIFICATION OR BE UNDER ESCORT BY PROPERLY BADGED PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL EMPLOYEES WITH A PERMANENT RECORD MAINTAINED ON EACH INDIVIDUAL TO WHOM A BADGE IS ISSUED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET.

THE CONTRACTOR AND ITS STAFF ARE RESPONSIBLE FOR COMPLETING SECURITY BADGE APPLICATIONS AND ATTENDING (AND PASSING) A SECURITY BADGE TRAINING CLASS CONDUCTED BY THE AIRPORT. PAPERWORK SHALL BE SUBMITTED IN ADVANCE AS DIRECTED BY AIRPORT SECURITY BEFORE ISSUANCE OF ANY BADGE. A NON-REFUNDABLE \$60 PROCESSING FEE WILL BE REQUIRED FOR EACH NEW BADGE. THIS FEE WILL BE PAID BEFORE ANY BADGE IS ISSUED.

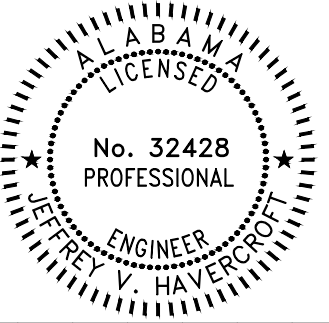
AT THE COMPLETION OF THE CONTRACT ALL BADGES MUST BE RETURNED TO THE AIRPORT. A FEE/CHARGE WILL BE ASSESSED FOR ALL BADGES NOT RETURNED. THE CONTRACTOR SHALL PLAN ON BADGING ONE EMPLOYEE FOR EVERY THREE EMPLOYEES ON SITE.

CONTRACTOR PERSONNEL ARE REQUIRED TO WEAR THEIR BADGES ABOVE THEIR WAISTS AT ALL TIMES WHEN WORKING ON THE AIRPORT. IDENTIFIABLE HARD HATS, REFLECTIVE VESTS/OUTERWEAR, OR OTHER IDENTIFICATION SHALL ALSO BE WORN AS REQUIRED BY AIRPORT OPERATIONS.

7. SECURITY IDENTIFICATION - VEHICLES: THE CONTRACTOR, THROUGH THE CSSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. ALL VEHICLES SHALL BE MARKED AND COMPLY WITH THE SAFETY REQUIREMENTS OUTLINED IN THESE PROJECT DOCUMENTS.
8. EMPLOYEE PARKING AREA: NO CONTRACTOR EMPLOYEE PERSONAL VEHICLES WILL BE ALLOWED WITHIN THE AOA. CONTRACTOR EMPLOYEE PARKING MUST BE IN LOCATION DESIGNATED BY THE AIRPORT OR AS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022).
9. FINES: PAYMENT OF ALL FINES ASSESSED TO THE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FEDERAL AVIATION ADMINISTRATION (FAA) AND/OR TRANSPORTATION SECURITY ADMINISTRATION (TSA) SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

NOTES:

1. THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR TEN (10) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.



DATE	----	3/27/2025	4/10/2025		
DESCRIPTION	NOT FOR CONSTRUCTION	ISSUED FOR REVIEW / COMMENT	FINAL DESIGN		
MARK					

PROJECT NUMBER:	25-E-01-03100	ISSUE DATE:	--/--/2025	SCALE:	1" : 500"
DESIGNED BY:	JH	DRAWN BY:	DJB		

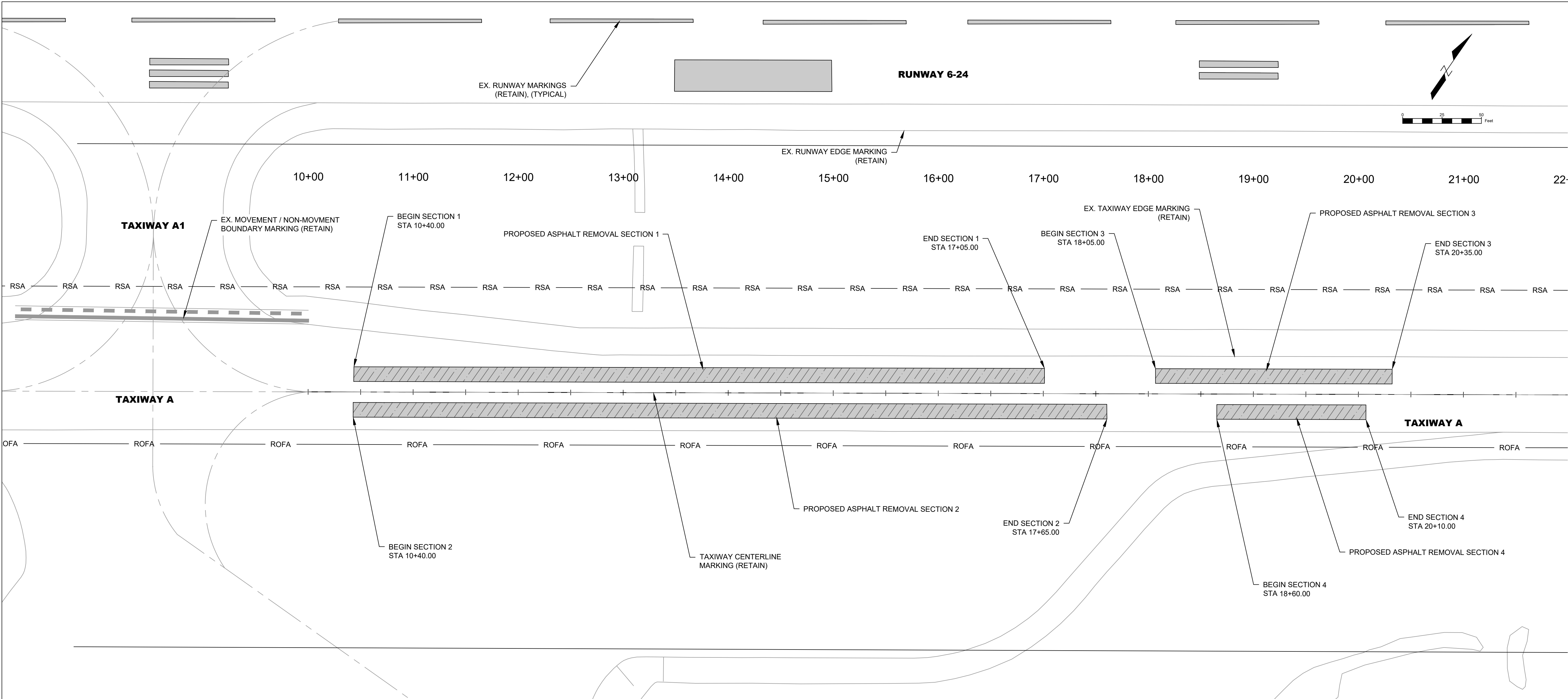
SARCOR, LLC

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INTERATIONAL AIRPORT  
TAXIWAY A PAVEMENT  
REHABILITATION  
5900 MESSER AIRPORT HWY  
BIRMINGHAM AL 35212

SHEET  
C-3.0  
PHASING PLAN





GENERAL CONTRACT NOTES:

- CHANGES: ONLY ENGINEER APPROVAL IS AUTHORIZED TO MAKE CHANGES IN THE CONTRACT THAT WILL AFFECT THE CONTRACT AMOUNT, AND THOSE REQUESTS MUST BE DONE THROUGH THE CHANGE ORDER SYSTEM.
- COMMUNICATION: PROPER COMMUNICATIONS IS ESSENTIAL TO THE SUCCESS OF THE PROJECT. ALL COMMUNICATIONS WILL BE HANDLED IN THE FOLLOWING MANNER. A REQUEST FROM A SUBCONTRACTOR WILL BE GIVEN TO THE CONTRACTOR. HE WILL EVALUATE IT AND DETERMINE IF THE OWNER NEEDS TO BE RESPONSIVE. IF SO, HE WILL GIVE IT TO THE RPR, WHO WILL GIVE IT TO AIRPORT OPERATIONS, WHO WILL DISSEMINATE IT TO THE APPROPRIATE AIRPORT DIVISION FOR A RESPONSE. THE RETURN INFORMATION WILL FOLLOW DOWN THE SYSTEM TO THE CONTRACTOR FOR HIS ACTION. EXCEPTION - IN CASE OF AN EMERGENCY, DIRECT COMMUNICATION IS AUTHORIZED TO QUICKLY RESOLVE THE SITUATION. ONCE STABILIZED, THE CONSTRUCTION TEAM (CONTRACTOR AND OWNER) WILL BE NOTIFIED OF THE SITUATION AND WHAT ACTIONS HAVE BEEN TAKEN.
- HAUL ROUTES: LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY THE OWNER, ENGINEER, OR RPR. TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED LOW PROFILE BARRICADES OR ORANGE CONSTRUCTION FENCE. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE REQUIRED AND APPROVED BY AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE AND OBTAIN ANY NECESSARY HAUL PERMITS AS REQUIRED BY THE LOCAL JURISDICTION.  
  
-- ALL ON-SITE ACCESS ROADS SHALL REMAIN OPEN, CLEAR OF ALL MATERIALS AND EQUIPMENT, AND MAINTAINED AT ALL TIMES. ANY SERVICE, ACCESS, OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE. ANY DAMAGE MUST BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME. ANY PAVEMENTS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED TO THE OWNER'S SATISFACTION AND AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME.
- SITE ACCESS: THE CONTRACTOR SHALL ACCESS THE PROJECT SITE THROUGH AN EXISTING OR TEMPORARY GATE AT THE LOCATION SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). THE CONTRACTOR SHALL NOT ATTEMPT TO GAIN ACCESS TO THE AIRFIELD THROUGH ANY OTHER GATE OR BUILDING, UNLESS GIVEN PERMISSION IN WRITING FROM THE AIRPORT. ANY DAMAGE TO EXISTING OR TEMPORARY ACCESS GATES AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ITS PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER.  
  
- ALL CONSTRUCTION TRAFFIC TO/FROM STAGING AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT ONCE INSIDE AOA SECURITY FENCE. IT IS ANTICIPATED THAT THERE WILL BE ONE AIRPORT OPERATIONS VEHICLE ASSIGNED TO CONSTRUCTION TRAFFIC ESCORTING.  
  
- ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON ACTIVE AIRFIELD OPERATIONS AREAS.
- STAGING AREA: ALL CONSTRUCTION MATERIALS, EQUIPMENT, BARRICADES, ETC. MUST BE STORED IN THE STAGING AREA SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). NO CONSTRUCTION EQUIPMENT OR MATERIALS WILL BE ALLOWED TO REMAIN IN CONSTRUCTION AREAS OR NEAR CONSTRUCTION ENTRANCES WHILE CONTRACTOR IS NOT ON-SITE. ALL CONSTRUCTION VEHICLES, EQUIPMENT, MATERIALS, ETC. MUST REMAIN A MINIMUM 10 FEET AWAY FROM ALL AIRPORT SECURITY/PERIMETER FENCES.

- CONTRACTOR UTILITIES: STAGING AREAS DO NOT HAVE UTILITIES. ANY UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANY OR AUTHORITY AND SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.  
  
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY RESTROOM FACILITIES. FOR ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL. THE CONTRACTOR SHALL NOT USE AIRPORT OR TENANT RESTROOM FACILITIES. TEMPORARY RESTROOM FACILITIES MUST BE LOCATED IN THE CONTRACTOR STAGING AREA, UNLESS OTHERWISE APPROVED BY THE AIRPORT.
- CONSTRUCTION SIGNAGE: THE CONTRACTOR SHALL INSTALL CONSTRUCTION/TRAFFIC CONTROL SIGNAGE AT THE LOCATIONS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). REFER TO SHEET G011 FOR ADDITIONAL SIGN DETAILS. THE CONTRACTOR SHALL COMPLY WITH ALL STATE/LOCAL REGULATIONS.
- PERMITS: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION AND EQUIPMENT.
- SITE UTILITIES: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES OR AIRPORT UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT NO ADDITIONAL COST TO THE OWNER. FAA TECH OPS WILL FLAG ANY FAA OWNED CABLES WITHIN THE PROJECT SITE LIMITS. CONTRACTOR SHALL COORDINATE WITH FAA TECH OPS TO FLAG FAA OWNED CABLES PRIOR TO THE START OF CONSTRUCTION.
- TEMPORARY DRAINAGE: THROUGHOUT THE COURSE OF CONSTRUCTION, THE CONTRACTOR MUST PROVIDE AND INSTALL ANY DRAINAGE PROVISIONS NECESSARY TO MAINTAIN POSITIVE (CONTINUOUS AND FLOWING) DRAINAGE AND NOT RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT END OF PROJECT CONTRACTOR MUST RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO OWNER.
- WASTE DISPOSAL: ALL PAVEMENT MILLINGS, RUBBLE, WASTE MATERIAL, AND OTHER DEBRIS SHALL BE LEGALLY DISPOSED OF OFF AIRPORT PROPERTY. ALL ON-SITE EXCESS AND/OR STORED MATERIAL NOT CLAIMED BY THE AIRPORT MUST BE REMOVED FROM AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
- COORDINATION OF CONSTRUCTION ACTIVITIES: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE RPR. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR MUST BE REVIEWED AND APPROVED BY THE RPR. THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE OWNER.
- COORDINATION OF CONSTRUCTION FACILITIES: THE CONTRACTOR MAY BE REQUIRED TO SHARE STAGING AREAS, STOCKPILE AREAS, AND CONSTRUCTION ACCESS POINTS WITH OTHER CONTRACTORS COMPLETING CAPITAL IMPROVEMENT PROJECTS AT THE AIRPORT. COORDINATION OF THE USE OF THESE AREAS WILL BE THROUGH THE OWNER, AS NEEDED.

LEGEND:

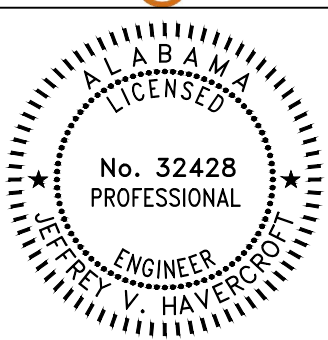
- |           |   |
|-----------|---|
|           | PROPOSED PAVEMENT REHABILITATION                          |
| — #TSA —  | EXISTING TAXIWAY SAFETY AREA (ADG AS INDICATED BY #)      |
| — #TOFA — | EXISTING TAXIWAY OBJECT FREE AREA (ADG AS INDICATED BY #) |
| — ROFA —  | RUNWAY OBJECT FREE AREA                                   |
| — RSA —   | RUNWAY SAFETY AREA  |
| — GS —    | GLIDE SLOPE CRITICAL AREA                                 |

NOTES:

- THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR SEVEN (7) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.

PAVEMENT REHAB SECTIONS

NO.	LENGTH	WIDTH
1	663	14
2	721	14
3	227	14
4	143	14



DATE	DESCRIPTION	MARK	PROJECT NUMBER:	DESIGNED BY:
---	NOT FOR CONSTRUCTION		25-E-01-03100	JH
3/27/2025	ISSUED FOR REVIEW / COMMENT		ISSUE DATE:	DRAWN BY:
4/10/2025	FINAL DESIGN		--/--/2025	DJB
			SCALE:	1" : 50"

SARCOR, LLC

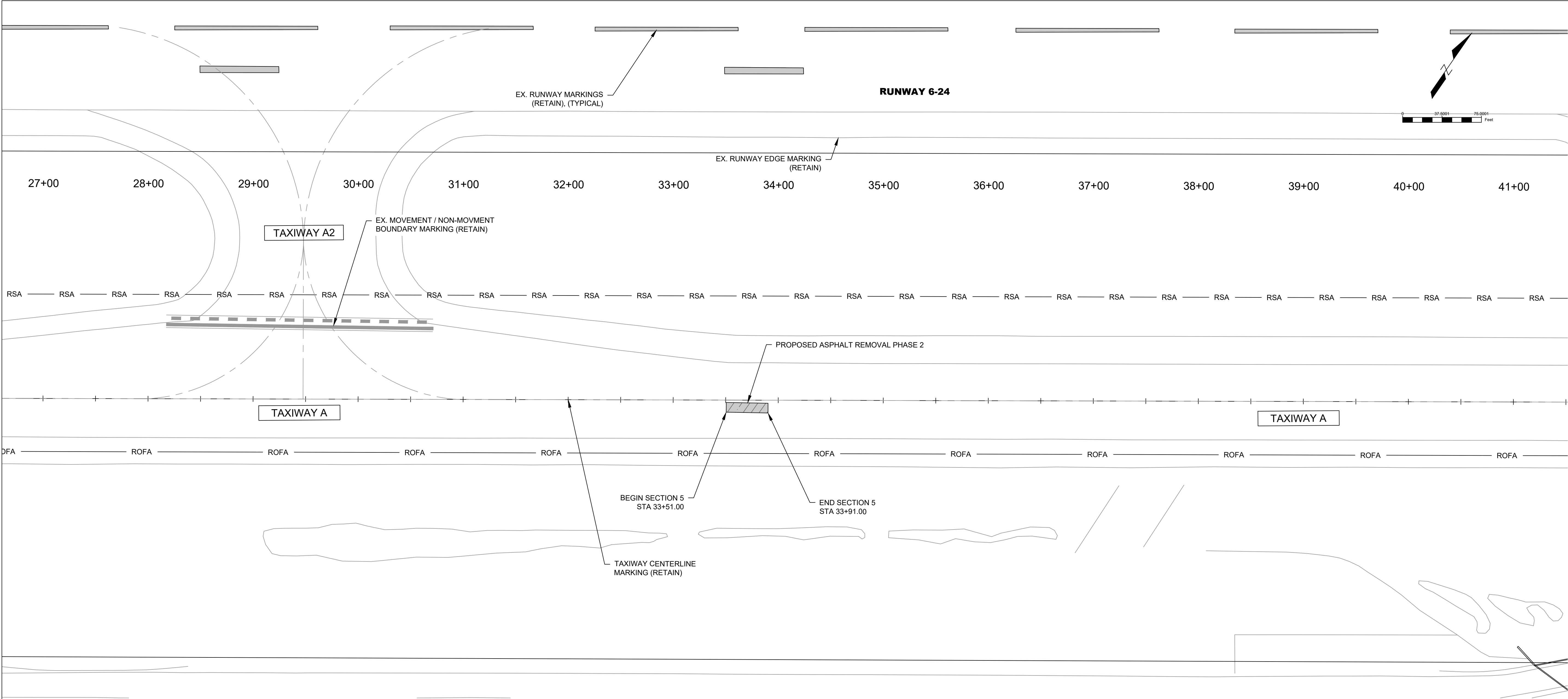
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BIRMINGHAM, AL 35203

BIRMINGHAM-  
SHUTTLESWORTH  
INTERATIONAL AIRPORT  
TAXIWAY A PAVEMENT  
REHABILITATION

5900 MESSER AIRPORT HWY  
BIRMINGHAM AL 35212

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PLAN





GENERAL CONTRACT NOTES:

- CHANGES: ONLY ENGINEER APPROVAL IS AUTHORIZED TO MAKE CHANGES IN THE CONTRACT THAT WILL AFFECT THE CONTRACT AMOUNT, AND THOSE REQUESTS MUST BE DONE THROUGH THE CHANGE ORDER SYSTEM.
- COMMUNICATION: PROPER COMMUNICATIONS IS ESSENTIAL TO THE SUCCESS OF THE PROJECT. ALL COMMUNICATIONS WILL BE HANDLED IN THE FOLLOWING MANNER. A REQUEST FROM A SUBCONTRACTOR WILL BE GIVEN TO THE CONTRACTOR. HE WILL EVALUATE IT AND DETERMINE IF THE OWNER NEEDS TO BE RESPONSIVE. IF SO, HE WILL GIVE IT TO THE RPR, WHO WILL GIVE IT TO AIRPORT OPERATIONS, WHO WILL DISSEMINATE IT TO THE APPROPRIATE AIRPORT DIVISION FOR A RESPONSE. THE RETURN INFORMATION WILL FOLLOW DOWN THE SYSTEM TO THE CONTRACTOR FOR HIS ACTION. EXCEPTION - IN CASE OF AN EMERGENCY, DIRECT COMMUNICATION IS AUTHORIZED TO QUICKLY RESOLVE THE SITUATION. ONCE STABILIZED, THE CONSTRUCTION TEAM (CONTRACTOR AND OWNER) WILL BE NOTIFIED OF THE SITUATION AND WHAT ACTIONS HAVE BEEN TAKEN.
- HAUL ROUTES: LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY THE OWNER, ENGINEER, OR RPR. TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED LOW PROFILE BARRICADES OR ORANGE CONSTRUCTION FENCE. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE REQUIRED AND APPROVED BY AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE AND OBTAIN ANY NECESSARY HAUL PERMITS AS REQUIRED BY THE LOCAL JURISDICTION.  
  
-- ALL ON-SITE ACCESS ROADS SHALL REMAIN OPEN, CLEAR OF ALL MATERIALS AND EQUIPMENT, AND MAINTAINED AT ALL TIMES. ANY SERVICE, ACCESS, OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE. ANY DAMAGE MUST BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME. ANY PAVEMENTS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED TO THE OWNER'S SATISFACTION AND AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME.
- SITE ACCESS: THE CONTRACTOR SHALL ACCESS THE PROJECT SITE THROUGH AN EXISTING OR TEMPORARY GATE AT THE LOCATION SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). THE CONTRACTOR SHALL NOT ATTEMPT TO GAIN ACCESS TO THE AIRFIELD THROUGH ANY OTHER GATE OR BUILDING, UNLESS GIVEN PERMISSION IN WRITING FROM THE AIRPORT. ANY DAMAGE TO EXISTING OR TEMPORARY ACCESS GATES AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ITS PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER.  
  
- ALL CONSTRUCTION TRAFFIC TO/FROM STAGING AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT ONCE INSIDE AOA SECURITY FENCE. IT IS ANTICIPATED THAT THERE WILL BE ONE AIRPORT OPERATIONS VEHICLE ASSIGNED TO CONSTRUCTION TRAFFIC ESCORTING.  
  
- ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON ACTIVE AIRFIELD OPERATIONS AREAS.
- STAGING AREA: ALL CONSTRUCTION MATERIALS, EQUIPMENT, BARRICADES, ETC. MUST BE STORED IN THE STAGING AREA SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). NO CONSTRUCTION EQUIPMENT OR MATERIALS WILL BE ALLOWED TO REMAIN IN CONSTRUCTION AREAS OR NEAR CONSTRUCTION ENTRANCES WHILE CONTRACTOR IS NOT ON-SITE. ALL CONSTRUCTION VEHICLES, EQUIPMENT, MATERIALS, ETC. MUST REMAIN A MINIMUM 10 FEET AWAY FROM ALL AIRPORT SECURITY/PERIMETER FENCES.

- CONTRACTOR UTILITIES: STAGING AREAS DO NOT HAVE UTILITIES. ANY UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANY OR AUTHORITY AND SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.  
  
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY RESTROOM FACILITIES. FOR ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL. THE CONTRACTOR SHALL NOT USE AIRPORT OR TENANT RESTROOM FACILITIES. TEMPORARY RESTROOM FACILITIES MUST BE LOCATED IN THE CONTRACTOR STAGING AREA, UNLESS OTHERWISE APPROVED BY THE AIRPORT.
- CONSTRUCTION SIGNAGE: THE CONTRACTOR SHALL INSTALL CONSTRUCTION/TRAFFIC CONTROL SIGNAGE AT THE LOCATIONS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). REFER TO SHEET G011 FOR ADDITIONAL SIGN DETAILS. THE CONTRACTOR SHALL COMPLY WITH ALL STATE/LOCAL REGULATIONS.
- PERMITS: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION AND EQUIPMENT.
- SITE UTILITIES: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES OR AIRPORT UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT NO ADDITIONAL COST TO THE OWNER. FAA TECH OPS WILL FLAG ANY FAA OWNED CABLES WITHIN THE PROJECT SITE LIMITS. CONTRACTOR SHALL COORDINATE WITH FAA TECH OPS TO FLAG FAA OWNED CABLES PRIOR TO THE START OF CONSTRUCTION.
- TEMPORARY DRAINAGE: THROUGHOUT THE COURSE OF CONSTRUCTION, THE CONTRACTOR MUST PROVIDE AND INSTALL ANY DRAINAGE PROVISIONS NECESSARY TO MAINTAIN POSITIVE (CONTINUOUS AND FLOWING) DRAINAGE AND NOT RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT END OF PROJECT CONTRACTOR MUST RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO OWNER.
- WASTE DISPOSAL: ALL PAVEMENT MILLINGS, RUBBLE, WASTE MATERIAL, AND OTHER DEBRIS SHALL BE LEGALLY DISPOSED OF OFF AIRPORT PROPERTY. ALL ON-SITE EXCESS AND/OR STORED MATERIAL NOT CLAIMED BY THE AIRPORT MUST BE REMOVED FROM AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
- COORDINATION OF CONSTRUCTION ACTIVITIES: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE RPR. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR MUST BE REVIEWED AND APPROVED BY THE RPR. THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE OWNER.
- COORDINATION OF CONSTRUCTION FACILITIES: THE CONTRACTOR MAY BE REQUIRED TO SHARE STAGING AREAS, STOCKPILE AREAS, AND CONSTRUCTION ACCESS POINTS WITH OTHER CONTRACTORS COMPLETING CAPITAL IMPROVEMENT PROJECTS AT THE AIRPORT. COORDINATION OF THE USE OF THESE AREAS WILL BE THROUGH THE OWNER, AS NEEDED.

LEGEND:

- #-TSA-

#-TOFA-

- ROFA -

- RSA -

- GS -
- PROPOSED PAVEMENT REHABILITATION

EXISTING TAXIWAY SAFETY AREA  
(ADG AS INDICATED BY #)

EXISTING TAXIWAY OBJECT FREE AREA  
(ADG AS INDICATED BY #)

RUNWAY OBJECT FREE AREA

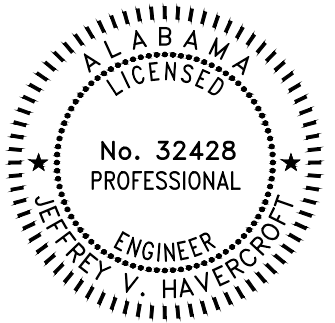

RUNWAY SAFETY AREA

GLIDE SLOPE CRITICAL AREA

NOTES:

- THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR THREE (3) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.

PAVEMENT REHAB SECTIONS		
NO.	LENGTH	WIDTH
5	40	10



DATE	---	3/27/2025	4/10/2025		
DESCRIPTION	NOT FOR CONSTRUCTION	ISSUED FOR REVIEW / COMMENT	FINAL DESIGN		
MARK					
PROJECT NUMBER:	25-E-01-03100		ISSUE DATE:	--/--/2025	
DESIGNED BY:	JH		DRAWN BY:	DJB	
			SCALE:	1" = 50"	

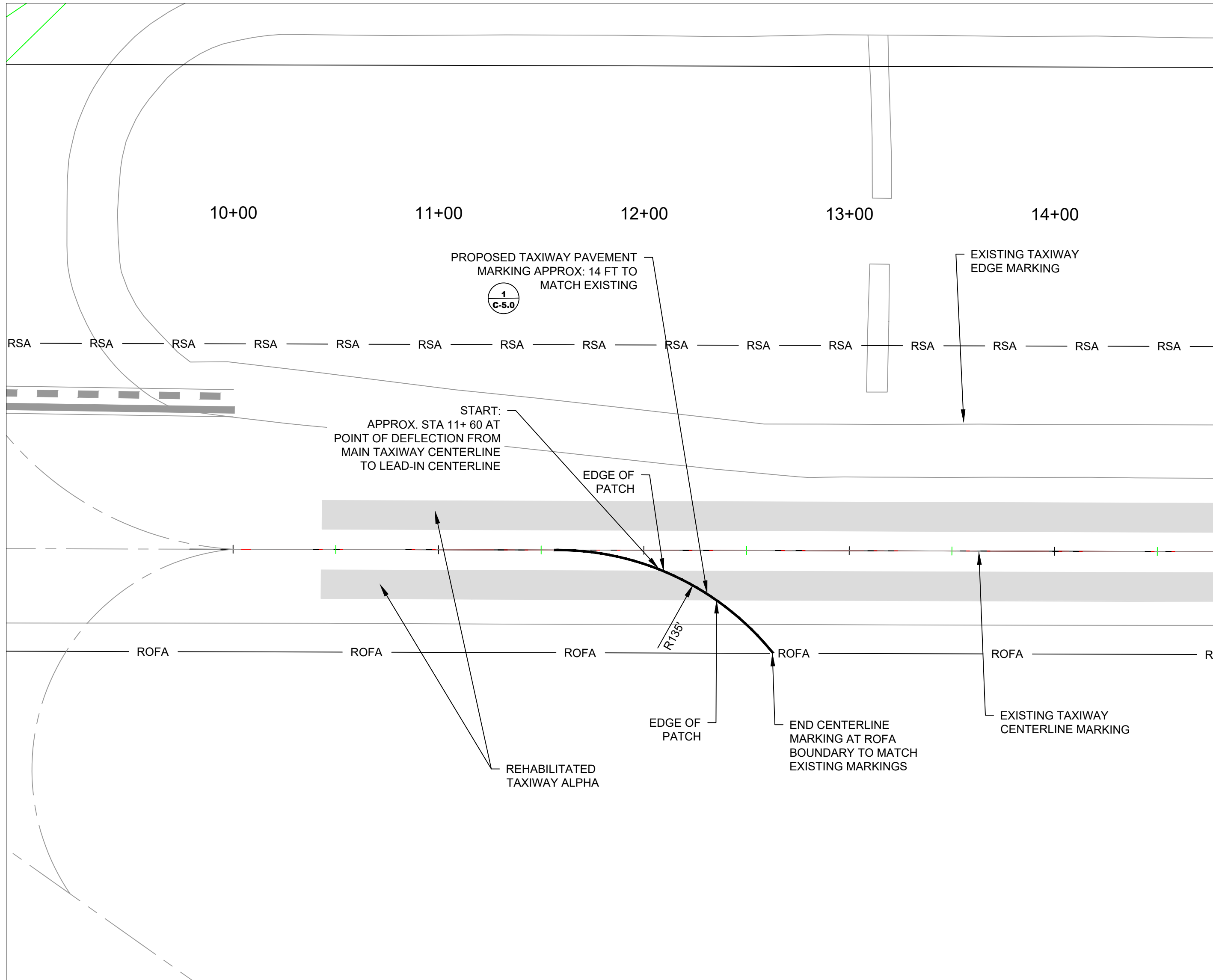
BIRMINGHAM-SHUTTLESWORTH  
INTERATIONAL AIRPORT  
TAXIWAY A PAVEMENT  
REHABILITATION  
5900 MESSER AIRPORT HWY  
BIRMINGHAM AL 35212

SARCOR, LLC

215 19 ST N, SUITE 101  
BIRMINGHAM, AL 35203

SHEET  
C-4.1  
REHABILITATION  
PLAN



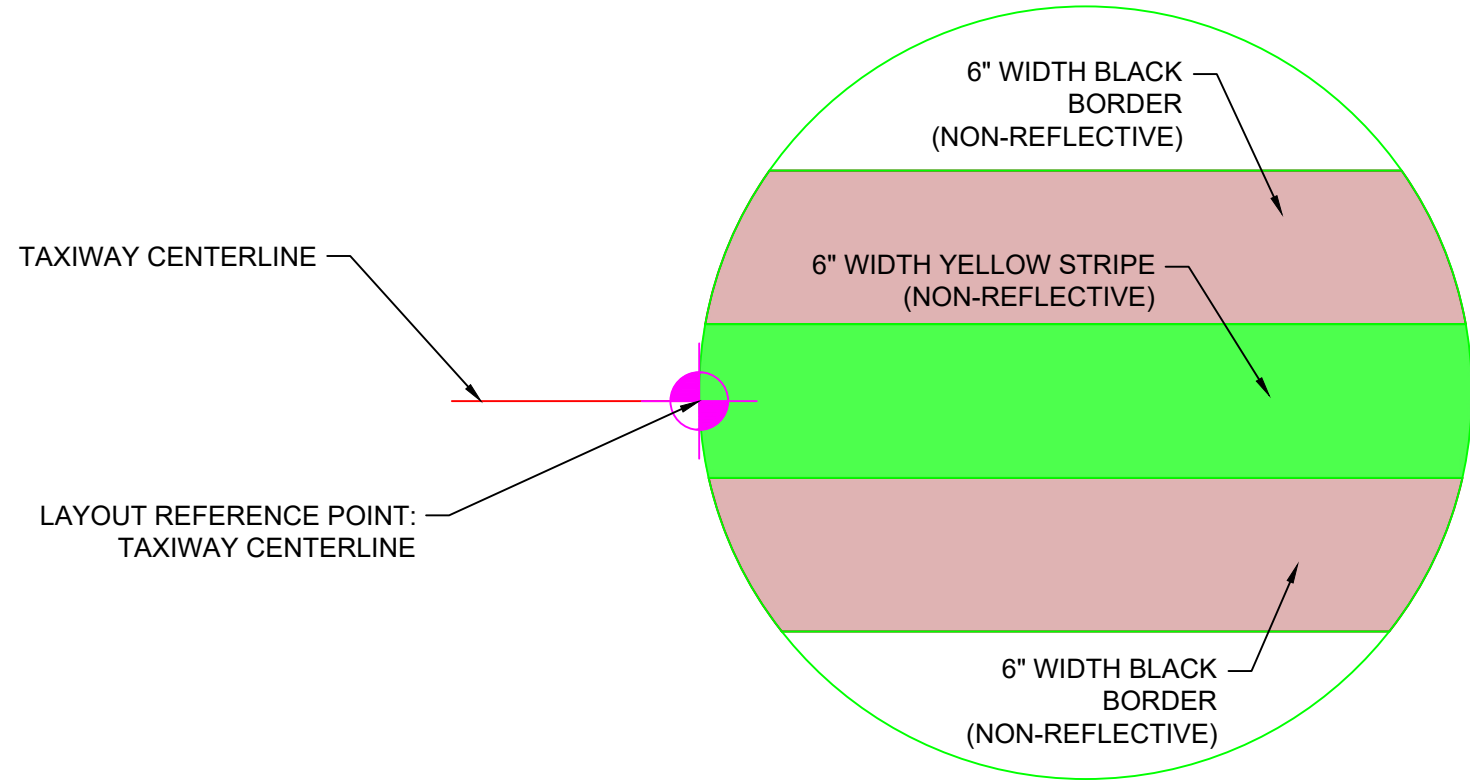


GENERAL CONTRACTOR NOTES:

- EXISTING TAXIWAY CENTERLINE MARKINGS SHALL BE PRESERVED IN PLACE. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO AVOID DAMAGE OR LOSS OF MARKINGS DURING PATCHING OPERATIONS.
- IF ANY PORTION OF THE EXISTING CENTERLINE MARKING IS DISTURBED OR REMOVED, THE CONTRACTOR SHALL RESTORE THE MARKING TO MATCH EXISTING IN COLOR (YELLOW WITH BLACK OUTLINE), WIDTH, AND ALIGNMENT AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL COORDINATE PATCH LAYOUT AND SAWCUTTING TO AVOID DISTURBANCE OF EXISTING MARKINGS WHERE POSSIBLE. CONTRACTOR SHALL DOCUMENT THE EXISTING MARKING LAYOUT PRIOR TO START OF WORK USING PHOTOGRAPHS, OFFSET MEASUREMENTS, OR OTHER APPROVED METHODS.
- ALL RESTORATION OF MARKINGS SHALL COMPLY WITH FAA AC 150/5370-10 AND AC 150/5340-1M (OR LATEST VERSION) FOR LAYOUT AND MATERIALS.

LEGEND:

- |            |  |
|------------|--|
| — #-TSA —  | EXISTING TAXIWAY SAFETY AREA<br>(ADG AS INDICATED BY #)      |
| — #-TOFA — | EXISTING TAXIWAY OBJECT FREE AREA<br>(ADG AS INDICATED BY #) |
| — ROFA —   | RUNWAY OBJECT FREE AREA                                      |
| — RSA —    | RUNWAY SAFETY AREA   |
| — GS —     | GLIDE SLOPE CRITICAL AREA                                    |



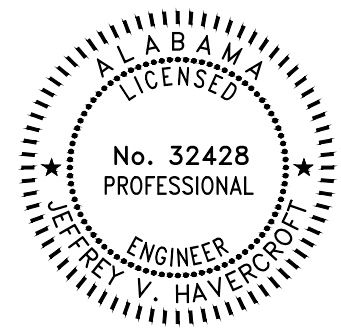
1 TAXIWAY CENTERLINE/LEAD-IN MARKING

SCALE: NTS

NOTES

- WHITE AND YELLOW PAINT SHALL BE USED. REFLECTORIZED GLASS BEADS PAINT SHALL NOT BE USED. SEE PAVEMENT MARKING PLAN FOR MARKING LOCATIONS.
- ALL TAXIWAY CENTERLINE MARKINGS ARE YELLOW.
- ALL PAINT MARKINGS SHALL BE OUTLINED WITH A 6" BLACK BORDER UNLESS OTHERWISE NOTED. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING WHEN BLACK OUTLINE IS PLACED AROUND MARKINGS (FIRST OR LAST).
- IF CONTRACTOR CHOOSES TO PAINT BLACK MARKINGS AFTER YELLOW HAS BEEN INSTALLED, CONTRACTOR SHALL NOT ALLOW BLACK PAINT OVERSPRAY ONTO THE YELLOW MARKINGS AND THERE SHALL BE CLEAN STRAIGHT LINES FOR ALL MARKINGS.

**THIS DOCUMENT SHOULD BE PRINTED IN COLOR**  
IMPORTANT DESIGN INFORMATION MAY NOT BE CORRECTLY INTERPRETED NOR  
EASILY DECIPHERED IF PRINTED IN GRAYSCALE



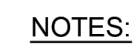
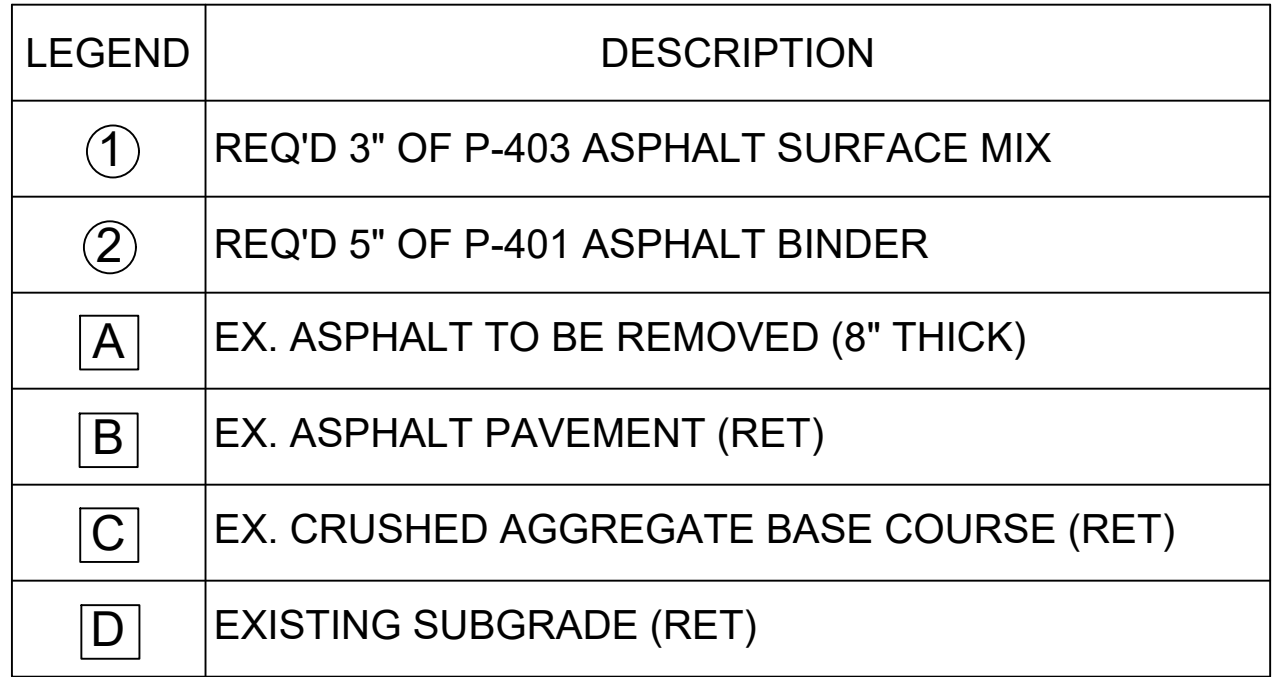
DATE	DESCRIPTION	MARK	PROJECT NUMBER:	DESIGNED BY:	DRAWN BY:	SCALE:
----	NOT FOR CONSTRUCTION		25-E-01-03100	JH		
3/27/2025	ISSUED FOR REVIEW / COMMENT		ISSUE DATE:		DJB	
4/10/2025	FINAL DESIGN		--/--/2025			

**SARCOR, LLC**  
215 19 ST N, SUITE 101  
BIRMINGHAM, AL 35203

BIRMINGHAM-  
SHUTTLESWORTH  
INTERATIONAL AIRPORT  
TAXIWAY A PAVEMENT  
REHABILITATION  
5900 MESSER AIRPORT HWY  
BIRMINGHAM AL 35212

SHEET  
C-5.0  
MARKING PLAN





1. SAFETY FLAG MUST BE PROMINENTLY DISPLAYED ON ALL CONSTRUCTION EQUIPMENT.

## SCALE: NTS



1. ALL COSTS SHALL BE INCIDENTAL TO ITEM G-105, "TEMPORARY CONSTRUCTION ITEMS".
2. MARKER SHALL BE YELLOW AND CONSTRUCTED OF VINYL OR OTHER MATERIAL APPROVED BY THE OWNER AND ENGINEER. TEMPORARY PAINT WILL NOT BE ALLOWED.
3. MARKER MATERIAL SHALL BE SECURELY ANCHORED TO THE PAVEMENT (SAND BAGS OR OTHER AIRPORT APPROVED METHOD) TO PREVENT DISLOCATION BY WIND OR JET/PROP BLAST. CONTRACTOR SHALL REPAIR PAVEMENT DAMAGED DURING ANY ANCHORING OF CLOSURE MARKINGS.
4. SEE DETAILED PHASING PLANS FOR MARKER LOCATIONS. THE CONTRACTOR SHALL COORDINATE THE EXACT PLACEMENT AND LOCATION OF ALL TAXIWAY CLOSURE MARKINGS WITH THE RPR AND AIRPORT OPERATIONS.

## SCALE: NTS



SCALE: NTS



**2** SCALE: NTS



## **S2 SIGN LEGEND**

### **S3 SIGN LEGEND**

SCALE: NTS

- NOTES:
1. CONTRACTOR MUST FURNISH, INSTALL, AND MAINTAIN TEMPORARY CONSTRUCTION TRAFFIC SIGNS AS INDICATED ON THE PLANS.
  2. S1 SIGNS MUST BE LOCATED A MIN. 300 FEET IN EACH DIRECTION FROM THE INTERSECTION WITH THE CONSTRUCTION ENTRANCE.
  3. SEE SHEETS XXXX FOR SIGN LOCATIONS. THE CONTRACTOR SHALL COORDINATE THE EXACT PLACEMENT AND LOCATION OF ALL CONSTRUCTION TRAFFIC SIGNS WITH THE RPR AND AIRPORT OPERATIONS.
  4. SIGN LETTERS ARE TO BE BLACK, MINIMUM HEIGHT OF 3", AND BE A BOLD LETTERING STYLE.
  5. SIGN MOUNTING PER ALDOT STANDARD SPECIFICATIONS.
  6. ARROW DIRECTIONS ON SIGNS MAY VARY.
  7. ALL COSTS SHALL BE INCIDENTAL TO ITEM G-105, "TEMPORARY CONSTRUCTION ITEMS".