# BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT TAXIWAY A PAVEMENT REHABILITATION

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SHEET

TITLE SHEET

**OWNER** 

BIRMINGHAM AIRPORT AUTHORITY (BAA) 5900 MESSER AIRPORT HWY BIRMINGHAM, AL 35212

CONTACT: SCHAVASS L.. HAMILTON, DBA, C.M.

**CIVIL ENGINEER** 

SARCOR, LLC 215 19TH STREET SOUTH BIRMINGHAM, AL 35203

CONTACT: JEFFREY HAVERCROFT, P.E. JEFF@SARCORLLC.COM

## **GENERAL NOTES**

- 4. THE CONTRACTOR SHALL SUPERVISE AND DIRECT THE PROJECT DESCRIBED HEREIN. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ALL CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES, AND FOR COORDINATING ALL PORTIONS OF WORK UNDER THE CONTRACT. EACH CONTRACTOR SHALL COOPERATE WITH THE OWNER'S REPRESENTATIVE, AND COORDINATE HIS WORK WITH THE WORK OF OTHERS
- THESE DRAWINGS ARE FORMATTED FOR 22" X 34". OTHER SIZE VERSIONS ARE NOT PRINTED TO THE SCALE CALLED OUT OR SHOWN.
- CONTRACTOR IS RESPONSIBLE FOR THE COST AND FEES ASSOCIATED WITH THE EXECUTION OF WORK. THIS INCLUDES ANY AND ALL PERMITS AND IMPACT FEES INCLUDING, BUT NOT LIMITED TO, DEMOLITION PERMITS, ADEM PERMIT FEES, BUILDING PERMITS, SANITARY SEWER FEES AND IMPACT FEES, WATER TAP FEES, ETC.
- B. IF THE CONTRACTOR DAMAGES ANY (EXISTING) SITE FEATURES DURING CONSTRUCTION, HE SHALL AT HIS OWN EXPENSE REPLACE OR REPAIR THE FEATURES IMMEDIATELY TO ORIGINAL CONDITION AND QUALITY AS APPROVED BY THE OWNER OR DESIGNATED REPRESENTATIVE.
- C. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL SAFETY CODES OF THE GOVERNING MUNICIPALITIES.
- D. DEVIATIONS FROM THESE PLANS AND ANY ASSOCIATED SPECIFICATIONS WITHOUT PRIOR WRITTEN CONSENT OF THE ENGINEER OF RECORD MAY CAUSE WORK TO BE
- E. WHEN APPLICABLE, FIRE DEPARTMENT ACCESS SHALL BE ALWAYS MAINTAINED.
- F. WHEN APPLICABLE, SUFFICIENT BARRICADES, LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES AND METHODS WHICH MAY BE NECESSARY FOR THE PUBLIC SAFETY AND PROTECTION SHALL BE IN ACCORDANCE WITH GOVERNING ORDINANCES AND M.U.T.C.D. (CURRENT EDITION) AND SHALL BE PROVIDED AND MAINTAINED THROUGHOUT

# SHEET INDEX

C-1.0 TITLE SHEET

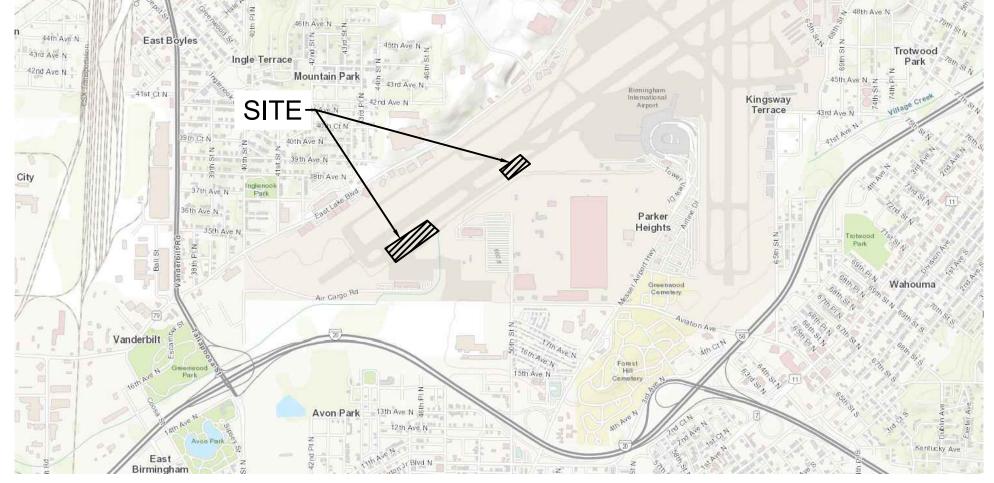
C-2.0 PROJECT NOTES

C-3.0 PHASING PLAN

C-4.0 REHABILITATION PLAN C-4.1 REHABILITATION PLAN

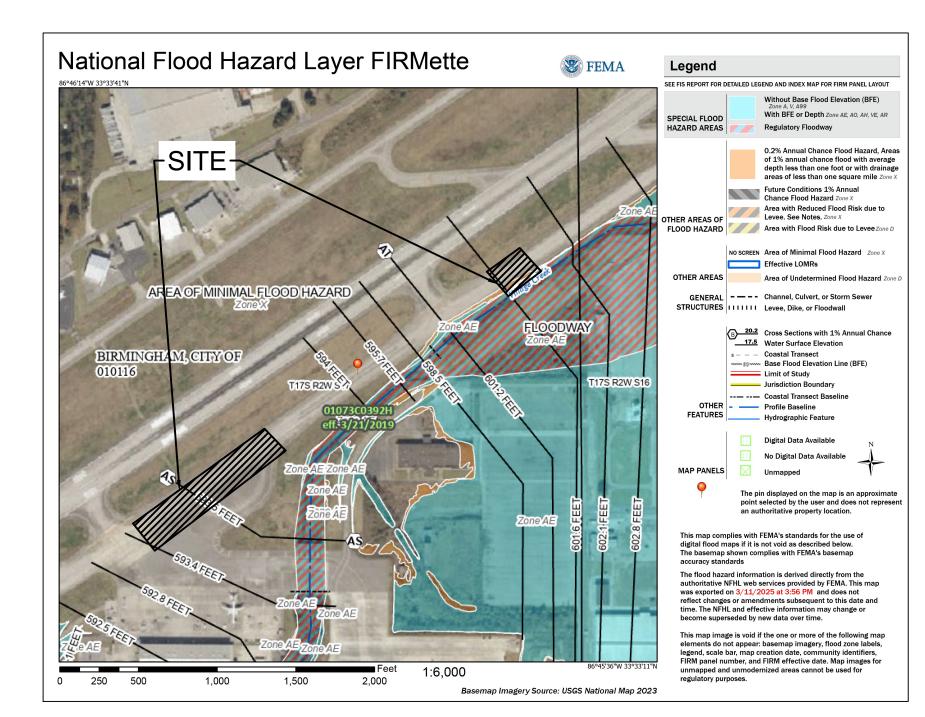
C-5.0 MARKING PLAN

C-6.0 TYPICAL SECTION & DETAIL SHEET



# **VICINITY MAP**

N.T.S PARCEL IDENTIFICATION: 23 00 17 1 000 006.000 & 23 00 17 4 000 001.000



# FEMA MAP

EFFECTIVE DATE 03/21/2019 SITE IS LOCATED WITHIN FLOOD ZONE X

THERE IS NO FLOODPLAIN ON THIS PROPERTY AS PER FIRM PANEL 01073C0392H DATED 03/21/2019. NO WORK IS BEING DONE WITHIN A FLOODPLAIN. THE PROPERTY SHOWN HEREON LIES WITHIN ZONE X AND IS NOT WITHIN A SPECIAL FLOOD HAZARD AREA.

# PROJECT NOTES

## **CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS**

1. SAFETY IS PARAMOUNT: IF ANY OF THE CONTRACTOR'S PERSONNEL OR A MEMBER OF THE AIRPORT STAFF SEES AN UNSAFE SITUATION ON THE PROJECT, THEY ARE OBLIGATED TO STOP THE WORK, RESTORE A SAFE CONDITION. AND NOTIFY A SUPERVISOR OF THE PROBLEM. AT THAT POINT THE RPR. AIRPORT REPRESENTATIVE, AND THE CONTRACTOR WILL MEET, RESOLVE THE SITUATION, AND CONTINUE THE WORK

THE AIRPORT SHALL, AT ALL TIMES, HAVE COMPLETE JURISDICTION OVER THE SAFETY AND SECURITY OF ALL AIRCRAFT OPERATIONS DURING THE WORK. WHEREVER THE SAFETY AND SECURITY TO AIR TRAFFIC IS CONCERNED, THE DECISIONS OF THE AIRPORT, OR ITS REPRESENTATIVE, SHALL BE FINAL AS TO THE METHODS, PROCEDURES, AND MEASURES USED.

- 2. CONSTRUCTION SAFETY PHASING PLAN COMPLIANCE: THE CONTRACTOR SHALL PROVIDE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) THAT OUTLINES HOW THE CONTRACTOR WILL COMPLY WITH THE EQUIREMENTS OF THE CONSTRUCTION SAFETY PHASING PLAN (CSPP). COST OF PREPARING THE SPCD SHALL BE INCLUSIVE OF BID ITEM G-102, "SAFETY AND SECURITY".
- 3. CONFORMANCE: THE CONTRACTOR SHALL CONFORM WITH ALL FEDERAL, STATE, AND LOCAL SAFETY REGULATIONS AND GUIDELINES AS SET FORTH IN FEDERAL AVIATION ADMINISTRATION (FAA) AC NO. 150/5370-2 SERIES, ALONG WITH THESE PLANS AND SPECIFICATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RPR, AIRPORT CONSTRUCTION MANAGER, AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SAFETY/SECURITY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON SAFETY REQUIREMENTS FOR THE CONTRACT.

THE CONTRACTOR, OR CSSO, SHALL THOROUGHLY ACQUAINT ALL SUPERVISORS AND EMPLOYEES WORKING AT THE AIRPORT ON THE IMPORTANCE OF FOLLOWING ESTABLISHED SAFETY AND SECURITY PROCEDURES THAT ARE INHERENT TO THIS ACTIVE AIR CARRIER AIRPORT. ADDITIONALLY, THE CONTRACTOR SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY.

4. CONTRACTOR VEHICLES: ALL CONTRACTOR AND SUBCONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE PROJECT SHALL BE REQUIRED TO HAVE THEIR COMPANY LOGO ON BOTH SIDES OF THE VEHICLE AND A FLASHING AMBER LIGHT OR A 3' X 3' ORANGE AND WHITE CHECKERBOARD FLAG (SEE DETAIL 1, SHEET G011). ALL VEHICLES SHALL HAVE AIRPORT-ISSUED VEHICLE IDENTIFICATION PRIOR TO ENTERING THE AIR OPERATIONS AREA. ANY VEHICLE OPERATING ON THE PROJECT DURING THE HOURS OF DARKNESS MUST BE EQUIPPED WITH A FLASHING AMBER LIGHT. MOUNTED ON TOP OF THE VEHICLE. AND OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES. LIGHTS AND FLAGS MUST BE MOUNTED AT THE UPPERMOST PART OF THE VEHICLE OR EQUIPMENT, SUCH THAT IT CAN BE VISIBLE FROM ALL DIRECTIONS, INCLUDING FROM THE AIR.

NO CONTRACTOR VEHICLE SHALL ENTER ANY ACTIVE RUNWAY OR TAXIWAY WITHOUT PROPER ESCORT FROM AIRPORT OPERATIONS. ESCORTED VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED ABOVE. ALL AIRCRAFT TRAFFIC ON RUNWAYS, TAXIWAYS, AND APRONS SHALL HAVE PRIORITY OVER CONTRACTOR'S

COORDINATION OF WORK AREA CLOSURES: NO RUNWAY, TAXIWAY, APRON, OR AIRPORT ROADWAY SHALL BE CLOSED WITHOUT WRITTEN APPROVAL OF AIRPORT OPERATIONS. TO ENABLE NECESSARY "NOTICES TO AIR MISSIONS" (NOTAM), ADVISORIES TO AIRPORT SERVICES OR TENANTS, AND/OR COORDINATION WITH OTHER ONGOING AIRPORT CONSTRUCTION PROJECTS, A MINIMUM OF 72 HOURS WRITTEN NOTICE REQUESTING CLOSING SHALL BE DIRECTED TO AIRPORT OPERATIONS, WHO WILL COORDINATE THE

ANY CONSTRUCTION ACTIVITY WITHIN 250 FEET OF THE RUNWAY CENTERLINE OR 85.5 FEET FROM THE CENTERLINE OF A TAXIWAY, OR OPEN EXCAVATIONS IN EXCESS OF TWO INCHES DEEP WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED IN WRITING BY AIRPORT OPERATIONS.

- 6. STOCKPILED MATERIALS: STOCKPILED MATERIAL WITHIN THE AOA SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLAST, PROPELLERS, OR WIND CONDITIONS IN EXCESS OF TEN KNOTS. STOCKPILE HEIGHT SHALL BE LESS THAN 10 FEET. THE CONTRACTOR IS NOT PERMITTED TO STOCKPILE EARTHWORK MATERIAL WITHIN THE PROJECT LIMITS. ALL EXCESS MATERIAL THAT DOES NOT MATCH OR CONFORM TO EXISTING GRADES MUST BE HAULED TO THE CONTRACTOR'S STAGING AREA, STOCKPILE AREA, OR HAULED OFFSITE TO AN APPROVED FACILITY BY THE END OF EACH
- 7. OPEN TRENCHES: OPEN TRENCHES LOCATED IN THE SAFETY AREA OF OPEN RUNWAYS OR TAXIWAYS WILL NOT BE PERMITTED. OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIAL LOCATED IN THE AOA (OUTSIDE OF AN OPEN RSA OR TSA) SHALL BE PROMINENTLY MARKED WITH FLAGS AND LIGHTED BY APPROVED LOW-LEVEL LIGHTED UNITS (ACCEPTABLE TO THE OWNER) DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS. THE CONTRACTOR IS TO NOTIFY BAA 48 HOURS PRIOR TO ANY OPEN TRENCH EXCAVATION WORK. THE CONTRACTOR IS TO NOTIFY BAA WHEN OPEN TRENCH EXCAVATION IS COMPLETE AND THE EARTHWORK AND ASSOCIATED GRADES ARE RESTORED.
- 8. OPEN FLAME & BLASTING: OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS HAVE BEEN TAKEN AND THE PROCEDURE APPROVED BY THE RPR AND/OR AIRPORT. BLASTING IS PROHIBITED ON THE AIRPORT.
- 9. FOREIGN OBJECT DEBRIS (FOD): DEBRIS, WASTE, AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS, OR BEING INGESTED IN JET ENGINES SHALL BE PROPERLY CONTROLLED AND PICKED UP AT ALL TIMES. IF THESE MATERIALS ARE OBSERVED, THE CONTRACTOR SHALL REMOVE THEM IMMEDIATELY.
- 10. DUST AND DEBRIS CONTROL: THE CONTRACTOR SHALL KEEP THE PROJECT AREA FREE FORM LOOSE OR BLOWABLE DEBRIS AT ALL TIMES. MATERIALS SHALL BE SECURED SO THAT THEY WILL NOT BE BLOWN BY THE WIND ONTO AIRFIELD SURFACES. SPECIAL ATTENTION TO DUST CONTROL WILL BE REQUIRED WHEN EARTHWORK, PAVEMENT DEMOLITION, OR HAULING OPERATIONS ARE IN PROGRESS OR WHEN WIND AND SEVER WEATHER CONDITIONS CAUSE EXCESSIVE BLOWING OF DUST. IN THIS REGARD, THE CONTRACTOR SHALL APPLY WATER OR CALCIUM CHLORIDE SOLUTION TO THE AFFECTED SITES AS DIRECTED.

CONTRACTOR MUST PROVIDE A WORKING VACUUM OR BROOM TRUCK ONSITE AT ALL TIMES WORK IS BEING PERFORMED ON THE PROJECT, INCIDENTAL TO ITEM G-102, "SAFETY AND SECURITY". IF AN OPERATIONAL VACUUM OR BROOM TRUCK IS NOT ON-SITE, THE OWNER WILL NOT ALLOW THE CONTRACTOR TO BEGIN WORK ON-SITE AND NO EXTENSION OF CONTRACT TIME WILL BE GRANTED. ALL HAUL ROUTES AND AIRFIELD PAVEMENTS MUST BE SWEPT/VACUUMED CLEAN AND ACCEPTED BY AIRPORT OPERATIONS PRIOR TO THE CONTRACTOR LEAVING THE SITE AT THE END OF EACH WORK SHIFT.

11. SPILL & CONTAMINATION CONTROL: MEASURES SHALL BE ADOPTED TO PREVENT POTENTIAL POLLUTANTS FROM ENTERING ANY DRAINAGE SYSTEM OR WATERWAY. MATERIALS AND DEBRIS SHALL NOT BE STORED IN THE WORK AREA IN A MANNER THAT WOULD ALLOW THEM TO ENTER THE DRAINAGE SYSTEM AS A RESULT OF SPILLAGE, NATURAL RUNOFF, OR FLOODING. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE AIRPORT SHOULD THERE BE A SPILLAGE OF MATERIAL WHICH MIGHT CONTAMINATE THE DRAINAGE SYSTEM. IT SHALL ALSO BE THE CONTRACTOR'S RESPONSIBILITY TO REMOVE AND CLEAN UP SUCH SPILLAGE IN A MANNER ACCEPTABLE TO THE AIRPORT.

12. INSPECTION OF WORK AREAS: THE RPR AND AIRPORT OPERATIONS WILL INSPECT, PRIOR TO OPENING FOR AIRCRAFT USE, ANY RUNWAY OR TAXIWAY THAT HAS BEEN CLOSED FOR WORK, ON OR ADJACENT THERETO, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR.

13. FAA ADVISORY CIRCULARS: THE CONTRACTOR IS DIRECTED TO COMPLY WITH AND ACQUAINT HIS/HER EMPLOYEES WITH THE FOLLOWING SAFETY GUIDELINES, RELATED MATERIALS AND FAA ADVISORY

> 150/5200-18D "AIRPORT SAFETY-SELF INSPECTION" 150/5210-5D "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" 150/5370-2G "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"

COPIES OF THESE DOCUMENTS ARE AVAILABLE AT THE FOLLOWING WEBSITE: WWW.FAA.GOV/AIRPORTS/RESOURCES/ADVISORY CIRCULARS OR COPIES MAY BE OBTAINED FROM AIRPORT OPERATIONS. CONTRACTOR RESPONSIBLE FOR STAYING UP-TO-DATE WITH ANY UPDATES OR

- 14. CONSTRUCTION DELAYS: THE OWNER MAY HALT CONSTRUCTION DURING THE PROJECT AT ANY TIME IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT ACTIVITIES OR SAFETY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS. NECESSARY EXTENSIONS IN CONTRACT TIME WILL BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS, HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS.
- 15. AIR TRAFFIC CONTROL TOWER COMMUNICATIONS: ALL COMMUNICATION WITH THE AIR TRAFFIC CONTROL TOWER OR OTHER ELEMENTS OF THE AIRPORT SHALL BE THROUGH THE RPR AND/OR AIRPORT OPERATIONS.
- 16. NIGHTTIME/CONTINGENCY SAFETY PLAN: THE CONTRACTOR SHALL PREPARE A SAFETY PLAN SPECIFIC TO NIGHTTIME CONSTRUCTION OPERATIONS, AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS. THE CONTRACTOR SHALL ALSO PREPARE A DESTRUCTIVE WEATHER PLAN TO SET FORTH GENERAL GUIDANCE AND INFORMATION FOR THE CONTRACTOR TO COORDINATE PREPAREDNESS PLANS WHEN DESTRUCTIVE WEATHER THREATENS THE AIRPORT.
- 17. AIRFIELD LIGHTING AND SIGNAGE: WORK CLOSING RUNWAYS OR TAXIWAYS WILL REQUIRE THE CONTRACTOR TO COVER THE APPROPRIATE SIGN PANELS WITH BLACK PLASTIC AND THE APPROPRIATE AIRFIELD LIGHTING TO SHOW THAT THEY ARE NOT AVAILABLE FOR USE. AIRPORT PERSONNEL WILL ASSIST AND SHOW THE CONTRACTOR WHICH SIGNS AND AIRFIELD LIGHTING ARE AFFECTED.

## AOA GATE GUARD RESPONSIBILITIES

- 1. AOA GATE ACCESS PROCEDURE (EACH TIME ANY VEHICLE ENTERS)
- KEEP THE GATE CLOSED WHILE IT IS NOT BEING USED.

REVISIONS TO THE ADVISORY CIRCULARS LISTED ABOVE.

- MAKE POSITIVE CONTACT WITH THE DRIVER AND ANY PASSENGERS.
- INSPECT I.D.S, ENSURE THEY ARE VALID, AND CHECK ANY AIRPORT ISSUED BADGES AGAINST THE STOP LIST BEFORE ENTRY IS PERMITTED.
- VISUALLY INSPECT ALL STORAGE AREAS INSIDE AND OUTSIDE OF THE VEHICLE. ENSURE THERE ARE NO PROHIBITED ITEMS PRESENT INCLUDING FIREARMS - VERBALLY INQUIRE.
- IF THE DRIVER DOES NOT POSSESS AN AIRPORT BADGE. DO NOT PERMIT AOA ENTRY UNTIL AN AIRPORT BADGED PERSON WITH ESCORT PRIVILEGES HAS CONFIRMED THEY ARE TAKING OVER THE ESCORT OF THE NON-BADGED PERSON.

#### 2. AOA GATE GUARD GUIDELINES

- NO PERSONAL VEHICLES ARE PERMITTED TO ENTER THE AOA.
- ALL VEHICLES ENTERING THE AOA MUST DISPLAY APPROPRIATE COMPANY SIGNAGE ON BOTH SIDES OF
- GUARDS MUST ENSURE ALL NON-BADGED PERSONNEL ENTERING THE AOA POSSESS VALID I.D., EXPIRED
- NON-BADGED PERSONNEL MUST ALWAYS BE ESCORTED BY BADGED PERSONNEL WITH ESCORT PRIVILEGES.
- GUARDS MUST POSSESS THE MOST RECENTLY ISSUED STOP LIST.
- GUARDS MAY NEVER LEAVE AN UNLOCKED GATE UNATTENDED.
- ALL PERSONNEL ENTERING THROUGH THE GATE MUST BE CHECKED. THIS INCLUDES AIRPORT, TSA, FAA, AND CONTRACTOR/SUBCONTRACTOR PERSONNEL.
- GATE GUARDS MUST BE AWARE THEY ARE SUBJECT TO THE SAME SECURITY POSTURE TESTING AS THE

#### SECURITY

- 1. GENERAL INTENT: IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RPR AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SAFETY/SECURITY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT.
- 2. CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR AND SUB-CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION
- 3. ACCESS TO THE SITE: CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL TRAFFIC TO/FROM PROJECT CONSTRUCTION AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT AT ALL TIMES ONCE INSIDE AOA SECURITY FENCE. A CONTRACTOR GATE GUARD SHALL MONITOR AND COORDINATE ALL CONTRACTOR

ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE AOA SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE BADGED AND FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNAGE FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS.

- 4. MATERIALS DELIVERY TO THE SITE: ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE NAME "BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INAPPROPRIATELY. DELIVERIES MAY BE ESCORTED ON SITE BY A PROPERLY BADGED EMPLOYEE.
- 5. CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA, AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING, AND/OR WARNING DEVICES VISIBLE FOR DAY/NIGHT USE AT THE LOCATIONS SHOWN IN THE PLANS. TEMPORARY BARRICADES, FLAGGING, AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT
- 6. SECURITY IDENTIFICATION PERSONNEL: PRIOR TO BEGINNING WORK, ALL CONTRACTOR OR SUBCONTRACTOR EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. WHO WILL REQUIRE ACCESS TO, BE OPERATING VEHICLES OR EQUIPMENT ON, OR BE WORKING UNESCORTED ON THE AIRFIELD WILL BE REQUIRED TO OBTAIN AN AIRPORT ISSUED SECURITY IDENTIFICATION BADGE, IN ACCORDANCE WITH THE

ALL PERSONNEL MUST DISPLAY AIRPORT ISSUED IDENTIFICATION OR BE UNDER ESCORT BY PROPERLY BADGED PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL EMPLOYEES WITH A PERMANENT RECORD MAINTAINED ON EACH INDIVIDUAL TO WHOM A BADGE IS ISSUED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET.

THE CONTRACTOR AND ITS STAFF ARE RESPONSIBLE FOR COMPLETING SECURITY BADGE APPLICATIONS AND ATTENDING (AND PASSING) A SECURITY BADGE TRAINING CLASS CONDUCTED BY THE AIRPORT. PAPERWORK SHALL BE SUBMITTED IN ADVANCE AS DIRECTED BY AIRPORT SECURITY BEFORE ISSUANCE OF ANY BADGE. A NON-REFUNDABLE \$60 PROCESSING FEE WILL BE REQUIRED FOR EACH NEW BADGE. THIS FEE WILL BE PAID BEFORE ANY BADGE IS ISSUED.

AT THE COMPLETION OF THE CONTRACT ALL BADGES MUST BE RETURNED TO THE AIRPORT. A FEE/CHARGE WILL BE ASSESSED FOR ALL BADGES NOT RETURNED. THE CONTRACTOR SHALL PLAN ON BADGING ONE EMPLOYEE FOR EVERY THREE EMPLOYEES ON SITE.

CONTRACTOR PERSONNEL ARE REQUIRED TO WEAR THEIR BADGES ABOVE THEIR WAISTS AT ALL TIMES WHEN WORKING ON THE AIRPORT. IDENTIFIABLE HARD HATS, REFLECTIVE VESTS/OUTERWEAR, OR OTHER IDENTIFICATION SHALL ALSO BE WORN AS REQUIRED BY AIRPORT OPERATIONS.

- SECURITY IDENTIFICATION VEHICLES: THE CONTRACTOR, THROUGH THE CSSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. ALL VEHICLES SHALL BE MARKED AND COMPLY WITH THE SAFETY REQUIREMENTS OUTLINED IN THESE PROJECT DOCUMENTS.
- EMPLOYEE PARKING AREA: NO CONTRACTOR EMPLOYEE PERSONAL VEHICLES WILL BE ALLOWED WITHIN THE AOA. CONTRACTOR EMPLOYEE PARKING MUST BE IN LOCATION DESIGNATED BY THE AIRPORT OR AS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022).
- 9. FINES: PAYMENT OF ALL FINES ASSESSED TO THE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FEDERAL AVIATION ADMINISTRATION (FAA) AND/OR TRANSPORTATION SECURITY ADMINISTRATION (TSA) SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

#### FAA AND NAVAID FACILITIES:

- 1. THE CONTRACTOR, THRU THE RPR OR OTHER AIRPORT REPRESENTATIVE, SHALL NOTIFY FAA TECH OPS WELL IN ADVANCE OF ANY DIGGING AND CONSTRUCTION ACTIVITIES TO ALLOW THE FAA SUFFICIENT TIME TO LOCATE AND MARK ANY EXISTING FIELD UTILITIES WHICH MIGHT BY AFFECTED BY THIS PROJECT.
- 2. ANY FAA EQUIPMENT OR MISC. CABLE THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AS APPROVED BY FAA TECH OPS. ALL SPLICES AND/OR CABLE REPAIRS SHALL MEET FAA SPECIFICATIONS AND ACCOMPLISHED TO THE SATISFACTION OF FAA TECH OPS. ALL CABLE WORK SHALL BE PERFORMED BY QUALIFIED WORKMEN REGULARLY ENGAGED IN THIS TYPE OF WORK. IF EXISTING CABLE CANNOT BE REPAIRED TO THE SATISFACTION OF THE FAA, NEW CABLE OF LIKE KIND SHALL BE INSTALLED.
- 3. IF ANY FAA POWER, CONTROL, OR SIGNAL CABLES ARE DAMAGED, THE CONTRACTOR SHALL REPLACE THE CABLE IN ITS ENTIRETY. THE SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF
- 4. THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY IN THE EVENT OF DAMAGE TO FAA EQUIPMENT. ANY DAMAGE TO FAA FACILITIES, CABLES, OR ACCESS ROADS DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE FAA'S REQUIREMENTS, AND AT NOT ADDITIONAL EXPENSE TO THE AIRPORT/OWNER.
- 5. THE CONTRACTOR SHALL TAKE ALL NECESSARY STEPS TO PROTECT THE EXISTING AIRFIELD LIGHTING AND NAVIGATIONAL AID EQUIPMENT, CABLES, AND ASSOCIATED UTILITIES DURING CONSTRUCTION TO ASSURE CONTINUOUS OPERATION OF LIGHTS AND NAVIGATIONAL AIDS WHEN

#### CONSTRUCTION OPERATIONS:

- 1. SAFETY AND SECURITY SYSTEM: THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN ALL REQUIRED BARRICADES, SIGNS, DANGER SIGNALS, AND LIGHTS AT DESIGNATED LOCATIONS, HAVE ALL ACCESS GATES LOCKABLE AND GUARDED WITH BADGED AND BONDED PERSONNEL WHEN LEFT OPEN FOR USE BY THE CONTRACTOR, AND HAVE ALL EQUIPMENT FLAGGED AND FITTED WITH FLASHING AMBER LIGHTS ON TOP OF THE VEHICLES. ALL OF THESE ITEMS SHALL CONSIST OF THE SAFETY AND SECURITY SYSTEM FOR THE PROTECTION OF THE WORK AND THE SAFETY OF THE PUBLIC FOR BOTH LAND AND AIR TRAFFIC. THE CONTRACTOR SHALL INSTALL THE COMPONENTS OF THE SYSTEM AT THE APPROPRIATE TIMES AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE CONTRACT SHALL INSPECT EVERY ASPECT OF THE SAFETY AND SECURITY SYSTEM ON A DAILY BASIS AND ENSURE ALL COMPONENTS ARE FUNCTIONING PROPERLY. THE RPR SHALL ALSO CONDUCT DAILY INSPECTIONS OF THE SAFETY AND SECURITY SYSTEM.
- 2. FUELING: ANY TYPE OF FUELING SUPPORT FACILITY OR DEVICE USED TO REFUEL CONSTRUCTION FOUIPMENT IS SUBJECT TO COUNTY, CITY, AND AIRPORT FIRE INSPECTION, LOCAL FIRE CODES. AND SAFETY STANDARDS SHALL BE MET PRIOR TO COMMENCEMENT OF WORK. THE CONTRACT SHALL BE RESPONSIBLE FOR ARRANGING SUPPORT ACCESS AND SHALL HAVE A BADGED PARTY ACCOMPANYING FUEL SUPPORT PERSONNEL WHILE IN THE AOA. IN THE EVENT OF FUEL SPILLAGES, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT SO THE AIRPORT CAN TAKE APPROPRIATE SPILL PLAN ACTIONS. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE CLEAN-UP OF ANY SPILLS.
- 3. AIRFIELD DRIVING: VEHICLES SHALL YIELD RIGHT-OF-WAY TO ALL AIRCRAFT AT ALL TIMES. VEHICLES SHALL NOT EXCEED 10 MPH ON THE AIRCRAFT PARKING RAMPS OR 25 MPH ON THE PERIMETER/SERVICE ROADS. ALL VEHICLES ARE RESTRICTED TO 5 MPH WHEN IN CLOSE PROXIMITY TO AIRCRAFT. ANY PERSONS DAMAGING ANY AIRPORT FACILITY OR INVOLVED IN AN ACCIDENT ON THE AIRFIELD SHALL REPORT SUCH DAMAGE TO AIRPORT OPERATIONS IMMEDIATELY AND SHALL BE RESPONSIBLE FOR HIS/HER SHARE OF ANY COSTS REQUIRED TO REPAIR OR REPLACED THE DAMAGE FACILITY.
- 4. THE CONTRACTOR SHALL MAKE A CONCERTED EFFORT TO ENSURE ALL SAFETY AND SECURITY ITEMS ARE IN PROPER WORKING ORDER EACH DAY DUE TO THE HEIGHTENED SECURITY STATUS OF THE AIRPORT AND THE CONSIDERABLE LIABILITY ASSOCIATED WITH THE SAFETY AND SECURITY WORK. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO ENSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS THEIR OWN EQUIPMENT AND PERSONNEL. SPECIAL CONSIDERATIONS SHOULD BE GIVEN TO FLIGHT SCHEDULES AND MISCELLANEOUS AIRCRAFT OPERATIONS. THE CONTRACTOR SHALL OBEY ALL INSTRUCTIONS AS TO ROUTES TO BE TAKEN BY EQUIPMENT TRAVELING WITHIN THE AOA AND KEEP SUCH VEHICLES AND EQUIPMENT MARKED AS REQUIRED. THE CONTRACTOR SHALL MAKE THEIR OWN ESTIMATE OF ALL DIFFICULTIES TO BE
- 5. ENFORCEMENT: VIOLATION OF ANY AIRPORT SAFETY AND SECURITY RULES AND REGULATIONS, DEPENDING ON THE SEVERITY OF THE VIOLATION, MAY RESULT IN ONE OR MORE OF THE FOLLOWING: A VERBAL AND/OR WRITTEN WARNING; THE CONTRACT WORK BEING STOPPED UNTIL CORRECTIVE MEASURES ARE TAKEN TO PRECLUDE A RECURRENCE OF THE VIOLATIONS; RESTRICTION FROM ENTERING THE AIRPORT OPERATIONS AREA (AOA); OR REMOVAL FROM AIRPORT AND IMMEDIATE SURRENDERING OF ANY BADGE.

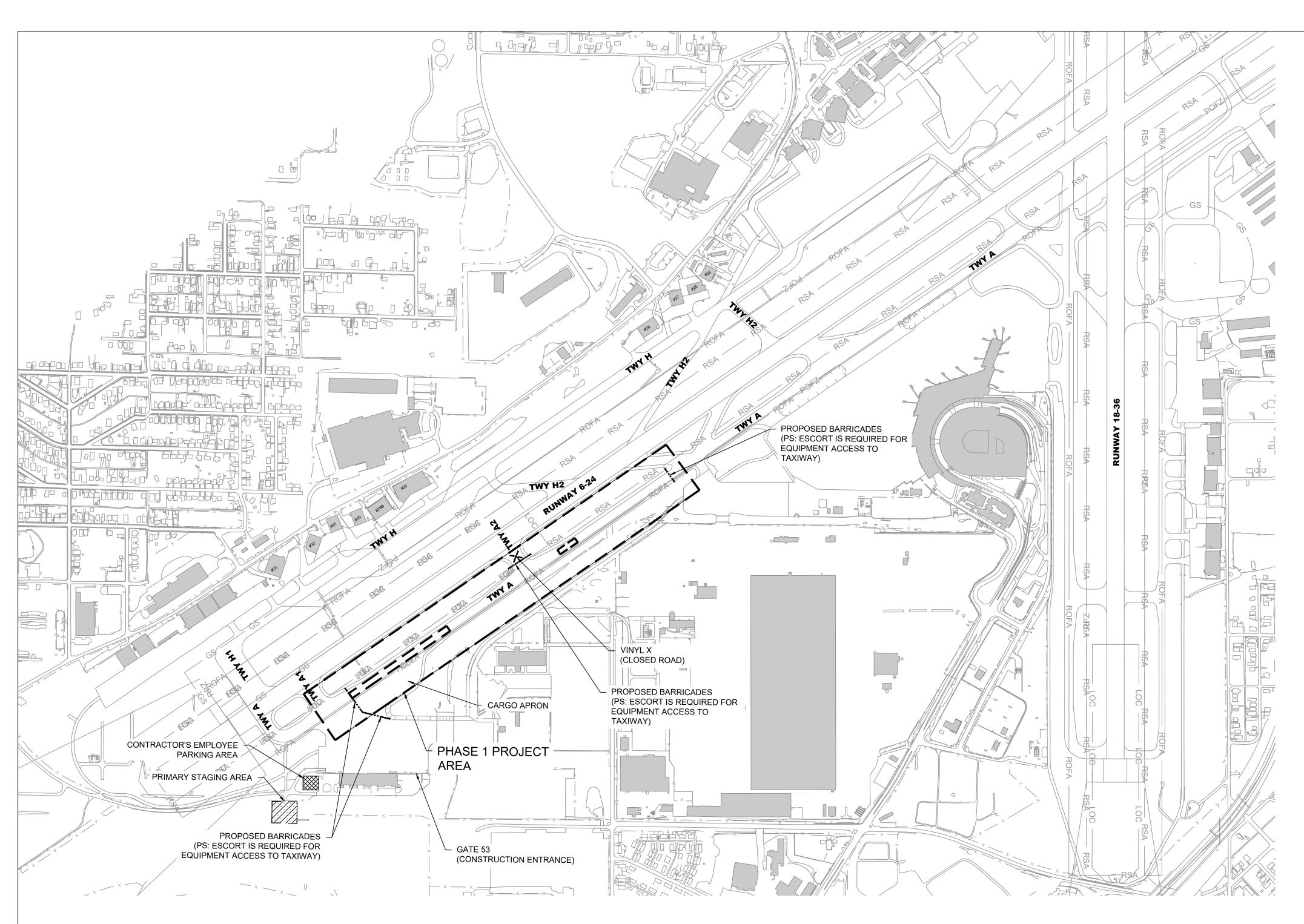
	SCHEDULE OF QUANTITIES		
ITEM NO.	DESCRIPTION	APPROX. QUANTITY	UNΠ
C-102.4	INSTALL CONSTRUCTION SITE SAFETY MEASURES	1	LSU
C-102.3	REMOVE CONSTRUCTION SITE SAFETY MEASURES	1	LSU
C-105	ESTABLISH SITE ACCESS AND MATERIAL STAGING AREAS	1	LSU
P-101	REMOVAL OF EXISTING PAVEMENT	2,728	SY
P-401	PLACEMENT OF BINDER ASPHALT LAYER 5" THICK	818	TON
P-403	PLACEMENT OF SURFACE WEARING LAYER 3" THICK	491	TON
P-620	PAVEMENT MARKING INCL. PRESERVATION OF CENTERLINE	1	LSU





PROJECT NUMBER:	MARK	DESCRIPTION	DATE
25-E-01-03100		NOT FOR CONSTRUCTION	1
		ISSUED FOR REVIEW / COMMENT	3/27/2025
SSUE DATE:		FINAL DESIGN	4/10/2025
//2025			
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SHEET



## AOA GATE GUARD RESPONSIBILITIES

- 1. AOA GATE ACCESS PROCEDURE (EACH TIME ANY VEHICLE ENTERS)
- KEEP THE GATE CLOSED WHILE IT IS NOT BEING USED.
- MAKE POSITIVE CONTACT WITH THE DRIVER AND ANY PASSENGERS.
- INSPECT I.D.S, ENSURE THEY ARE VALID, AND CHECK ANY AIRPORT ISSUED BADGES AGAINST THE STOP LIST BEFORE ENTRY IS PERMITTED.

VISUALLY INSPECT ALL STORAGE AREAS INSIDE AND OUTSIDE OF THE VEHICLE. ENSURE THERE ARE NO

- PROHIBITED ITEMS PRESENT INCLUDING FIREARMS VERBALLY INQUIRE.
- IF THE DRIVER DOES NOT POSSESS AN AIRPORT BADGE, DO NOT PERMIT AOA ENTRY UNTIL AN AIRPORT BADGED PERSON WITH ESCORT PRIVILEGES HAS CONFIRMED THEY ARE TAKING OVER THE ESCORT OF THE NON-BADGED PERSON.

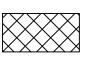
#### 2. AOA GATE GUARD GUIDELINES

- NO PERSONAL VEHICLES ARE PERMITTED TO ENTER THE AOA.
- ALL VEHICLES ENTERING THE AOA MUST DISPLAY APPROPRIATE COMPANY SIGNAGE ON BOTH SIDES OF
- GUARDS MUST ENSURE ALL NON-BADGED PERSONNEL ENTERING THE AOA POSSESS VALID I.D., EXPIRED I.D. IS INVALID.
- NON-BADGED PERSONNEL MUST ALWAYS BE ESCORTED BY BADGED PERSONNEL WITH ESCORT
- PRIVILEGES.
- GUARDS MUST POSSESS THE MOST RECENTLY ISSUED STOP LIST.
- GUARDS MAY NEVER LEAVE AN UNLOCKED GATE UNATTENDED.
- ALL PERSONNEL ENTERING THROUGH THE GATE MUST BE CHECKED. THIS INCLUDES AIRPORT, TSA, FAA, AND CONTRACTOR/SUBCONTRACTOR PERSONNEL.
- GATE GUARDS MUST BE AWARE THEY ARE SUBJECT TO THE SAME SECURITY POSTURE TESTING AS THE REST OF THE AIRPORT.

### **LEGEND:**



CONTRACTOR STAGING AREA



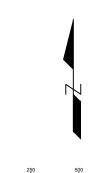
CONTRACTOR EMPLOYEE PARKING AREA

EXISTING TAXIWAY SAFETY AREA (ADG AS INDICATED BY #) EXISTING TAXIWAY OBJECT FREE AREA — #-TOFA — (ADG AS INDICATED BY #) RUNWAY OBJECT FREE AREA

RUNWAY SAFETY AREA

GLIDE SLOPE CRITICAL AREA

EXISTING FENCE



#### SECURITY

- GENERAL INTENT: IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RPR AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SAFETY/SECURITY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT.
- 2. CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR AND SUB-CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION
- 3. ACCESS TO THE SITE: CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL TRAFFIC TO/FROM PROJECT CONSTRUCTION AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT AT ALL TIMES ONCE INSIDE AOA SECURITY FENCE. A CONTRACTOR GATE GUARD SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT ANY AOA

ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE AOA SHALL BE LOCKED AND SECURED AT ALL GATE OPEN. IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE BADGED AND FAMILIAR WITH FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNAGE FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS.

- MATERIALS DELIVERY TO THE SITE: ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE NAME "BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INAPPROPRIATELY. DELIVERIES MAY BE ESCORTED ON SITE BY A PROPERLY BADGED EMPLOYEE
- CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING, AND/OR WARNING DEVICES VISIBLE FOR DAY/NIGHT USE AT THE LOCATIONS SHOWN IN THE PLANS. TEMPORARY BARRICADES, FLAGGING, AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT
- SECURITY IDENTIFICATION PERSONNEL: PRIOR TO BEGINNING WORK, ALL CONTRACTOR OR SUBCONTRACTOR EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. WHO WILL REQUIRE ACCESS TO, BE OPERATING VEHICLES OR EQUIPMENT ON, OR BE WORKING UNESCORTED ON THE AIRFIELD WILL BE REQUIRED TO OBTAIN AN AIRPORT ISSUED SECURITY IDENTIFICATION BADGE, IN ACCORDANCE WITH THE AIRPORT OPERATIONS SECURITY PROGRAM.

ALL PERSONNEL MUST DISPLAY AIRPORT ISSUED IDENTIFICATION OR BE UNDER ESCORT BY PROPERLY BADGED PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL ISSUED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET.

THE CONTRACTOR AND ITS STAFF ARE RESPONSIBLE FOR COMPLETING SECURITY BADGE APPLICATIONS AND ATTENDING (AND PASSING) A SECURITY BADGE TRAINING CLASS CONDUCTED BY THE AIRPORT. PAPERWORK SHALL BE SUBMITTED IN ADVANCE AS DIRECTED BY AIRPORT SECURITY BEFORE ISSUANCE OF ANY BADGE. A NON-REFUNDABLE \$60 PROCESSING FEE WILL BE REQUIRED FOR EACH NEW BADGE. THIS FEE WILL BE PAID BEFORE ANY BADGE IS ISSUED.

AT THE COMPLETION OF THE CONTRACT ALL BADGES MUST BE RETURNED TO THE AIRPORT. A FEE/CHARGE WILL BE ASSESSED FOR ALL BADGES NOT RETURNED. THE CONTRACTOR SHALL PLAN ON BADGING ONE EMPLOYEE FOR EVERY THREE EMPLOYEES ON SITE.

CONTRACTOR PERSONNEL ARE REQUIRED TO WEAR THEIR BADGES ABOVE THEIR WAISTS AT ALL TIMES WHEN WORKING ON THE AIRPORT. IDENTIFIABLE HARD HATS, REFLECTIVE VESTS/OUTERWEAR, OR OTHER IDENTIFICATION SHALL ALSO BE WORN AS REQUIRED BY AIRPORT OPERATIONS.

- SECURITY IDENTIFICATION VEHICLES: THE CONTRACTOR, THROUGH THE CSSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. ALL VEHICLES SHALL BE MARKED AND COMPLY WITH THE SAFETY REQUIREMENTS OUTLINED IN THESE PROJECT DOCUMENTS.
- 8. EMPLOYEE PARKING AREA: NO CONTRACTOR EMPLOYEE PERSONAL VEHICLES WILL BE ALLOWED WITHIN THE AOA. CONTRACTOR EMPLOYEE PARKING MUST BE IN LOCATION DESIGNATED BY THE AIRPORT OR AS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022).
- 9. FINES: PAYMENT OF ALL FINES ASSESSED TO THE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FEDERAL AVIATION ADMINISTRATION (FAA) AND/OR TRANSPORTATION SECURITY ADMINISTRATION (TSA) SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

#### **NOTES:**

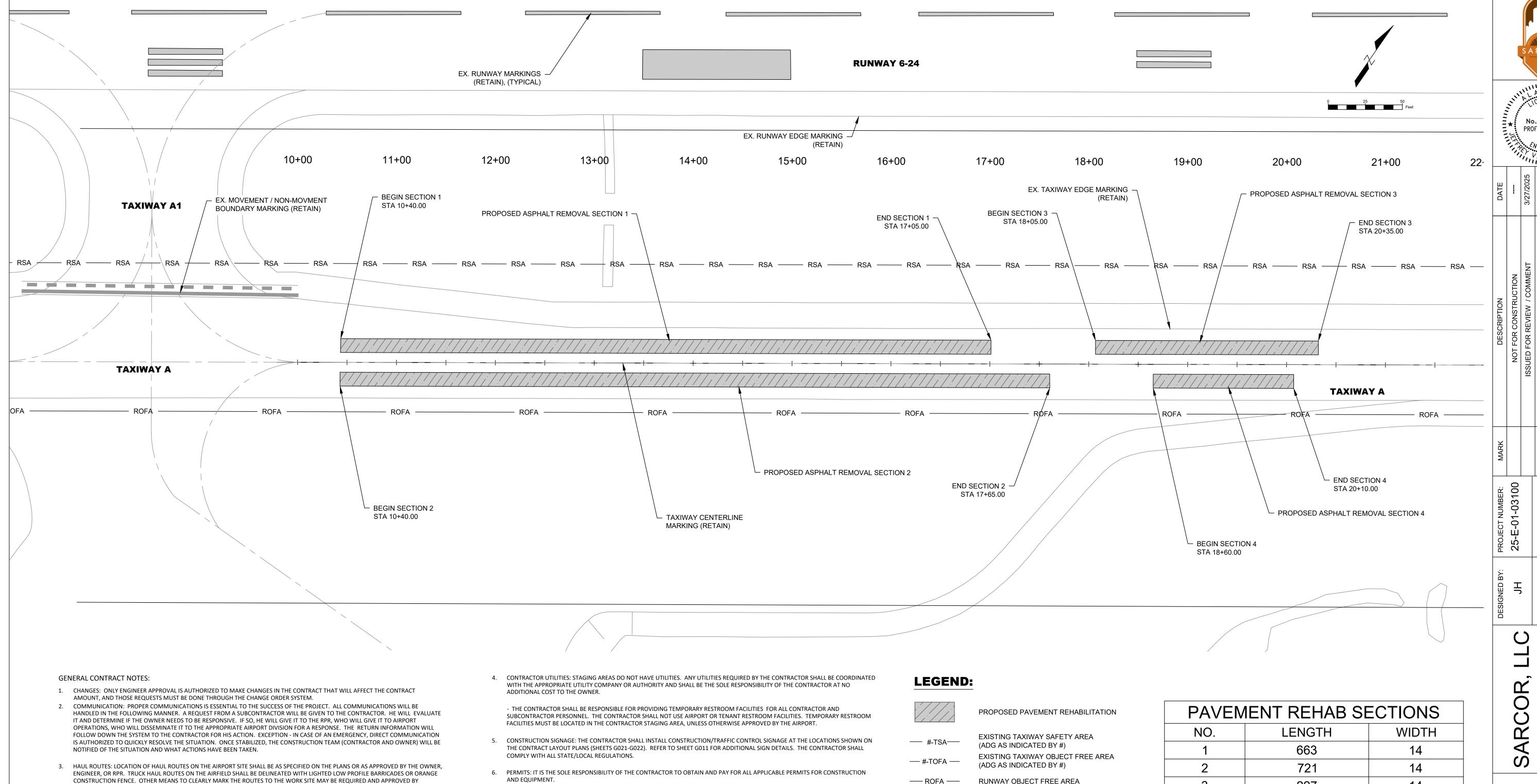
1. THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR TEN (10) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.



DATE		3/27/20;	4/10/202		
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SHEET C-3.0 PHASING PLAN



AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE AND OBTAIN

-- ALL ON-SITE ACCESS ROADS SHALL REMAIN OPEN, CLEAR OF ALL MATERIALS AND EQUIPMENT, AND MAINTAINED AT ALL TIMES. ANY

SERVICE, ACCESS, OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE. ANY DAMAGE MUST BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME. ANY PAVEMENTS DAMAGED

BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED TO THE OWNER'S SATISFACTION AND AT THE CONTRACTOR'S EXPENSE WITH NO

2. SITE ACCESS: THE CONTRACTOR SHALL ACCESS THE PROJECT SITE THROUGH AN EXISTING OR TEMPORARY GATE AT THE LOCATION SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). THE CONTRACTOR SHALL NOT ATTEMPT TO GAIN ACCESS TO THE AIRFIELD

THROUGH ANY OTHER GATE OR BUILDING, UNLESS GIVEN PERMISSION IN WRITING FROM THE AIRPORT. ANY DAMAGE TO EXISTING OR

- ALL CONSTRUCTION TRAFFIC TO/FROM STAGING AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT ONCE INSIDE AOA SECURITY

FENCE. IT IS ANTICIPATED THAT THERE WILL BE ONE AIRPORT OPERATIONS VEHICLE ASSIGNED TO CONSTRUCTION TRAFFIC ESCORTING.

- ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES.

3. STAGING AREA: ALL CONSTRUCTION MATERIALS, EQUIPMENT, BARRICADES, ETC. MUST BE STORED IN THE STAGING AREA SHOWN ON THE

CONTRACT LAYOUT PLANS (SHEETS G021-G022). NO CONSTRUCTION EQUIPMENT OR MATERIALS WILL BE ALLOWED TO REMAIN IN CONSTRUCTION AREAS OR NEAR CONSTRUCTION ENTRANCES WHILE CONTRACTOR IS NOT ON-SITE. ALL CONSTRUCTION VEHICLES, EQUIPMENT, MATERIALS, ETC. MUST REMAIN A MINIMUM 10 FEET AWAY FROM ALL AIRPORT SECURITY/PERIMETER FENCES.

ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON ACTIVE AIRFIELD OPERATIONS AREAS.

TEMPORARY ACCESS GATES AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ITS PRE-CONSTRUCTION CONDITION AT

ANY NECESSARY HAUL PERMITS AS REQUIRED BY THE LOCAL JURISDICTION.

ADDITIONAL COMPENSATION OR CONTRACT TIME.

NO ADDITIONAL COST TO THE OWNER.

- 7. SITE UTILITIES: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES OR AIRPORT UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT NO ADDITIONAL COST TO THE OWNER. FAA TECH OPS WILL FLAG ANY FAA OWNED CABLES WITHIN THE PROJECT SITE LIMITS. CONTRACTOR SHALL COORDINATE WITH FAA TECH OPS TO FLAG FAA OWNED CABLES PRIOR TO THE START OF CONSTRUCTION.
- 8. TEMPORARY DRAINAGE: THROUGHOUT THE COURSE OF CONSTRUCTION, THE CONTRACTOR MUST PROVIDE AND INSTALL ANY DRAINAGE PROVISIONS NECESSARY TO MAINTAIN POSITIVE (CONTINUOUS AND FLOWING) DRAINAGE AND NOT RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT END OF PROJECT CONTRACTOR MUST RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO OWNER.
- WASTE DISPOSAL: ALL PAVEMENT MILLINGS, RUBBLE, WASTE MATERIAL, AND OTHER DEBRIS SHALL BE LEGALLY DISPOSED OF OFF AIRPORT PROPERTY. ALL ON-SITE EXCESS AND/OR STORED MATERIAL NOT CLAIMED BY THE AIRPORT MUST BE REMOVED FROM AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
- 10. COORDINATION OF CONSTRUCTION ACTIVITIES: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE RPR. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR MUST BE REVIEWED AND APPROVED BY THE RPR. THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE OWNER.
- 11. COORDINATION OF CONSTRUCTION FACILITIES: THE CONTRACTOR MAY BE REQUIRED TO SHARE STAGING AREAS, STOCKPILE AREAS, AND CONSTRUCTION ACCESS POINTS WITH OTHER CONTRACTORS COMPLETING CAPITAL IMPROVEMENT PROJECTS AT THE AIRPORT. COORDINATION OF THE USE OF THESE AREAS WILL BE THROUGH THE OWNER, AS NEEDED.

	PROPOSED PAVEMENT REHABILITATION
#-TSA	EXISTING TAXIWAY SAFETY AREA (ADG AS INDICATED BY #)
— #-TOFA —	EXISTING TAXIWAY OBJECT FREE AREA (ADG AS INDICATED BY #)
— ROFA —	RUNWAY OBJECT FREE AREA
RSA	RUNWAY SAFETY AREA

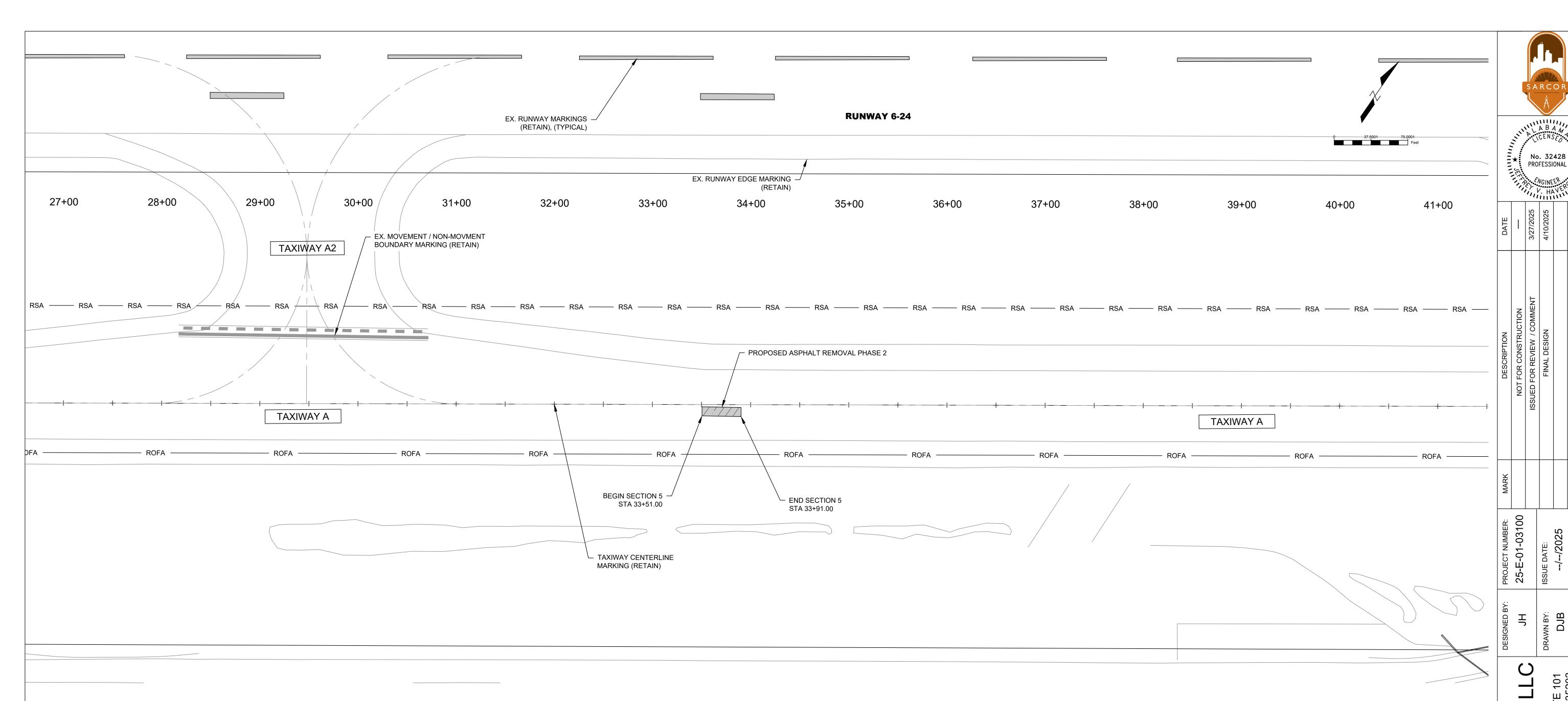
—— RSA ——	RUNWAY SAFETY AREA
—— GS ——	GLIDE SLOPE CRITICAL AREA

1. THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR SEVEN (7) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.

PAVEMENT REHAB SECTIONS				
NO.	LENGTH	WIDTH		
1	663	14		
2	721	14		
3	227	14		
4	143	14		

No. 32428 PROFESSIONAL

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DESCRIPTION		DAIR
NOT FOR CONSTRUCTION		
ISSUED FOR REVIEW / COMMENT	TN	3/27/202
FINAL DESIGN		4/10/202



#### **GENERAL CONTRACT NOTES:**

- 1. CHANGES: ONLY ENGINEER APPROVAL IS AUTHORIZED TO MAKE CHANGES IN THE CONTRACT THAT WILL AFFECT THE CONTRACT AMOUNT, AND THOSE REQUESTS MUST BE DONE THROUGH THE CHANGE ORDER SYSTEM.
- 2. COMMUNICATION: PROPER COMMUNICATIONS IS ESSENTIAL TO THE SUCCESS OF THE PROJECT. ALL COMMUNICATIONS WILL BE HANDLED IN THE FOLLOWING MANNER. A REQUEST FROM A SUBCONTRACTOR WILL BE GIVEN TO THE CONTRACTOR. HE WILL EVALUATE IT AND DETERMINE IF THE OWNER NEEDS TO BE RESPONSIVE. IF SO, HE WILL GIVE IT TO THE RPR, WHO WILL GIVE IT TO AIRPORT OPERATIONS, WHO WILL DISSEMINATE IT TO THE APPROPRIATE AIRPORT DIVISION FOR A RESPONSE. THE RETURN INFORMATION WILL FOLLOW DOWN THE SYSTEM TO THE CONTRACTOR FOR HIS ACTION. EXCEPTION - IN CASE OF AN EMERGENCY, DIRECT COMMUNICATION IS AUTHORIZED TO QUICKLY RESOLVE THE SITUATION. ONCE STABILIZED, THE CONSTRUCTION TEAM (CONTRACTOR AND OWNER) WILL BE NOTIFIED OF THE SITUATION AND WHAT ACTIONS HAVE BEEN TAKEN.
- 3. HAUL ROUTES: LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY THE OWNER, ENGINEER, OR RPR. TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED LOW PROFILE BARRICADES OR ORANGE CONSTRUCTION FENCE. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE REQUIRED AND APPROVED BY AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE AND OBTAIN ANY NECESSARY HAUL PERMITS AS REQUIRED BY THE LOCAL JURISDICTION.

-- ALL ON-SITE ACCESS ROADS SHALL REMAIN OPEN, CLEAR OF ALL MATERIALS AND EQUIPMENT, AND MAINTAINED AT ALL TIMES. ANY SERVICE, ACCESS, OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE. ANY DAMAGE MUST BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME. ANY PAVEMENTS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED TO THE OWNER'S SATISFACTION AND AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME.

- 2. SITE ACCESS: THE CONTRACTOR SHALL ACCESS THE PROJECT SITE THROUGH AN EXISTING OR TEMPORARY GATE AT THE LOCATION SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). THE CONTRACTOR SHALL NOT ATTEMPT TO GAIN ACCESS TO THE AIRFIELD THROUGH ANY OTHER GATE OR BUILDING, UNLESS GIVEN PERMISSION IN WRITING FROM THE AIRPORT. ANY DAMAGE TO EXISTING OR TEMPORARY ACCESS GATES AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED TO ITS PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER.
- ALL CONSTRUCTION TRAFFIC TO/FROM STAGING AREAS SHALL BE UNDER AIRPORT OPERATIONS ESCORT ONCE INSIDE AOA SECURITY FENCE. IT IS ANTICIPATED THAT THERE WILL BE ONE AIRPORT OPERATIONS VEHICLE ASSIGNED TO CONSTRUCTION TRAFFIC ESCORTING.
- ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON ACTIVE AIRFIELD OPERATIONS AREAS.
- 3. STAGING AREA: ALL CONSTRUCTION MATERIALS, EQUIPMENT, BARRICADES, ETC. MUST BE STORED IN THE STAGING AREA SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). NO CONSTRUCTION EQUIPMENT OR MATERIALS WILL BE ALLOWED TO REMAIN IN CONSTRUCTION AREAS OR NEAR CONSTRUCTION ENTRANCES WHILE CONTRACTOR IS NOT ON-SITE. ALL CONSTRUCTION VEHICLES, EQUIPMENT, MATERIALS, ETC. MUST REMAIN A MINIMUM 10 FEET AWAY FROM ALL AIRPORT SECURITY/PERIMETER FENCES.

- 4. CONTRACTOR UTILITIES: STAGING AREAS DO NOT HAVE UTILITIES. ANY UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY COMPANY OR AUTHORITY AND SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY RESTROOM FACILITIES FOR ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL. THE CONTRACTOR SHALL NOT USE AIRPORT OR TENANT RESTROOM FACILITIES. TEMPORARY RESTROOM FACILITIES MUST BE LOCATED IN THE CONTRACTOR STAGING AREA, UNLESS OTHERWISE APPROVED BY THE AIRPORT.
- CONSTRUCTION SIGNAGE: THE CONTRACTOR SHALL INSTALL CONSTRUCTION/TRAFFIC CONTROL SIGNAGE AT THE LOCATIONS SHOWN ON THE CONTRACT LAYOUT PLANS (SHEETS G021-G022). REFER TO SHEET G011 FOR ADDITIONAL SIGN DETAILS. THE CONTRACTOR SHALL COMPLY WITH ALL STATE/LOCAL REGULATIONS.
- 6. PERMITS: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION
- 7. SITE UTILITIES: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES OR AIRPORT UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT NO ADDITIONAL COST TO THE OWNER. FAA TECH OPS WILL FLAG ANY FAA OWNED CABLES WITHIN THE PROJECT SITE LIMITS. CONTRACTOR SHALL COORDINATE WITH FAA TECH OPS TO FLAG FAA OWNED CABLES PRIOR TO THE START OF CONSTRUCTION.
- 8. TEMPORARY DRAINAGE: THROUGHOUT THE COURSE OF CONSTRUCTION, THE CONTRACTOR MUST PROVIDE AND INSTALL ANY DRAINAGE PROVISIONS NECESSARY TO MAINTAIN POSITIVE (CONTINUOUS AND FLOWING) DRAINAGE AND NOT RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT END OF PROJECT CONTRACTOR MUST RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO OWNER.
- WASTE DISPOSAL: ALL PAVEMENT MILLINGS, RUBBLE, WASTE MATERIAL, AND OTHER DEBRIS SHALL BE LEGALLY DISPOSED OF OFF AIRPORT PROPERTY. ALL ON-SITE EXCESS AND/OR STORED MATERIAL NOT CLAIMED BY THE AIRPORT MUST BE REMOVED FROM AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
- 10. COORDINATION OF CONSTRUCTION ACTIVITIES: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND THE RPR. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR MUST BE REVIEWED AND APPROVED BY THE RPR. THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE OWNER.
- 11. COORDINATION OF CONSTRUCTION FACILITIES: THE CONTRACTOR MAY BE REQUIRED TO SHARE STAGING AREAS, STOCKPILE AREAS, AND CONSTRUCTION ACCESS POINTS WITH OTHER CONTRACTORS COMPLETING CAPITAL IMPROVEMENT PROJECTS AT THE AIRPORT. COORDINATION OF THE USE OF THESE AREAS WILL BE THROUGH THE OWNER, AS NEEDED.

#### **LEGEND:**



PROPOSED PAVEMENT REHABILITATION

EXISTING TAXIWAY SAFETY AREA (ADG AS INDICATED BY #) EXISTING TAXIWAY OBJECT FREE AREA

— #-TOFA — (ADG AS INDICATED BY #) RUNWAY OBJECT FREE AREA

RUNWAY SAFETY AREA

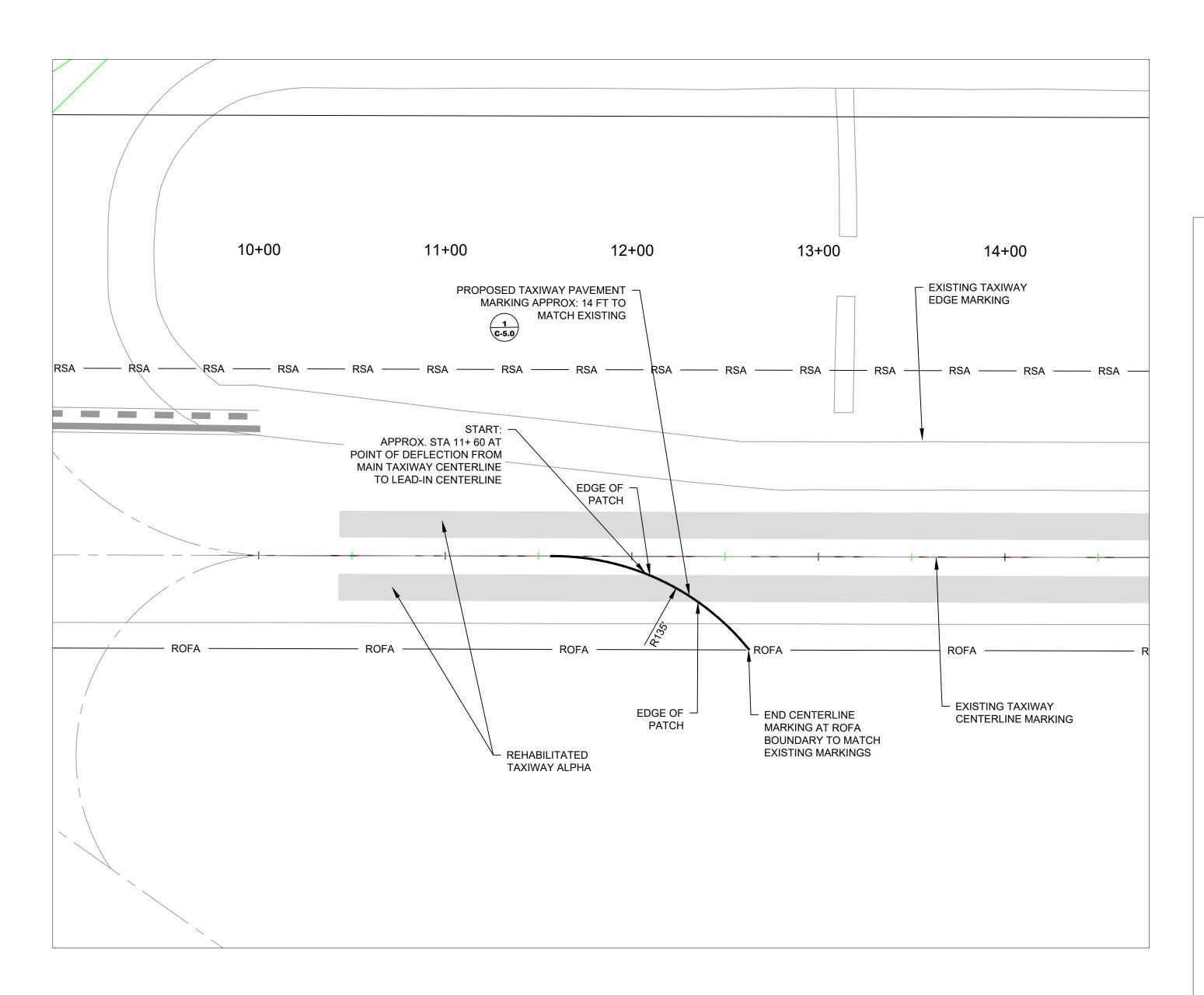
----- GS ----- GLIDE SLOPE CRITICAL AREA

#### **NOTES:**

1. THE CONTRACTOR SHALL COORDINATE WITH FAA TECHNICAL OPERATIONS, THE AIRPORT, AND THE RPR THREE (3) CALENDAR DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES IN ORDER TO PROVIDE SUFFICIENT NOTICE TO LOCATE AND MARK EXISTING BURIED CABLES, AND TO SCHEDULE ANY REQUIRED FACILITY OUTAGES.

PAVEMENT REHAB SECTIONS			
NO.	LENGTH	WIDTH	
5	40	10	

SHEET REHABILITATION

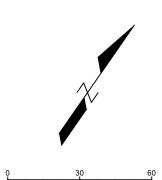


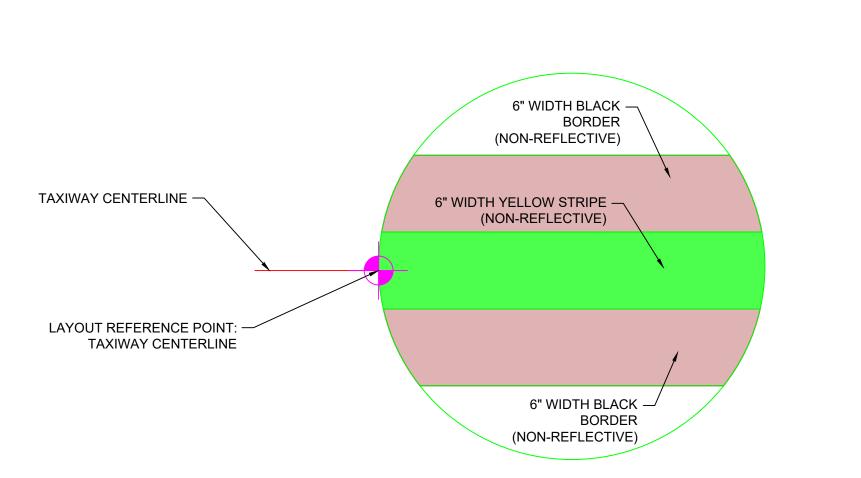
### GENERAL CONTRACTOR NOTES:

- EXISTING TAXIWAY CENTERLINE MARKINGS SHALL BE PRESERVED IN PLACE. CONTRACTOR SHALL
  TAKE ALL NECESSARY PRECAUTIONS TO AVOID DAMAGE OR LOSS OF MARKINGS DURING PATCHING
  OPERATIONS.
- 2. IF ANY PORTION OF THE EXISTING CENTERLINE MARKING IS DISTURBED OR REMOVED, THE CONTRACTOR SHALL RESTORE THE MARKING TO MATCH EXISTING IN COLOR (YELLOW WITH BLACK OUTLINE), WIDTH, AND ALIGNMENT AT NO ADDITIONAL COST TO THE OWNER.
- 3. CONTRACTOR SHALL COORDINATE PATCH LAYOUT AND SAWCUTTING TO AVOID DISTURBANCE OF EXISTING MARKINGS WHERE POSSIBLE. CONTRACTOR SHALL DOCUMENT THE EXISTING MARKING LAYOUT PRIOR TO START OF WORK USING PHOTOGRAPHS, OFFSET MEASUREMENTS, OR OTHER APPROVED METHODS.
- 4. ALL RESTORATION OF MARKINGS SHALL COMPLY WITH FAA AC 150/5370-10 AND AC 150/5340-1M (OR LATEST VERSION) FOR LAYOUT AND MATERIALS.

## **LEGEND:**

#-TSA	EXISTING TAXIWAY SAFETY AREA (ADG AS INDICATED BY #)
— #-TOFA —	EXISTING TAXIWAY OBJECT FREE ARE. (ADG AS INDICATED BY #)
— ROFA —	RUNWAY OBJECT FREE AREA
RSA	RUNWAY SAFETY AREA
GS	GLIDE SLOPE CRITICAL AREA





# TAXIWAY CENTERLINE/LEAD-IN MARKING SCALE: NTS

#### NOTE

- 1. WHITE AND YELLOW PAINT SHALL BE USED. REFLECTORIZED GLASS BEADS PAINT SHALL <u>NOT</u> BE USED. SEE PAVEMENT MARKING PLAN FOR MARKING LOCATIONS.
- 2. ALL TAXIWAY CENTERLINE MARKINGS ARE YELLOW.
- 3. ALL PAINT MARKINGS SHALL BE OUTLINED WITH A 6" BLACK BORDER UNLESS OTHERWISE NOTED. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING WHEN BLACK OUTLINE IS PLACED AROUND MARKINGS (FIRST OR LAST).
- 4. IF CONTRACTOR CHOOSES TO PAINT BLACK MARKINGS AFTER YELLOW HAS BEEN INSTALLED, CONTRACTOR SHALL NOT ALLOW BLACK PAINT OVERSPRAY ONTO THE YELLOW MARKINGS AND THERE SHALL BE CLEAN STRAIGHT LINES FOR ALL MARKINGS.

### THIS DOCUMENT SHOULD BE PRINTED IN COLOR

IMPORTANT DESIGN INFORMATION MAY NOT BE CORRECTLY INTERPRETED NOR EASILY DECIPHERED IF PRINTED IN GRAYSCALE



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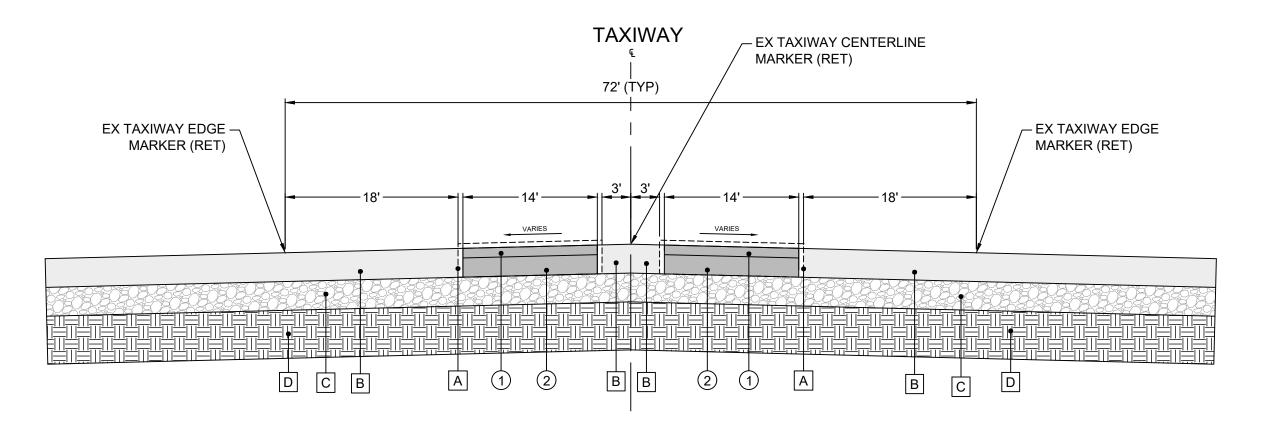
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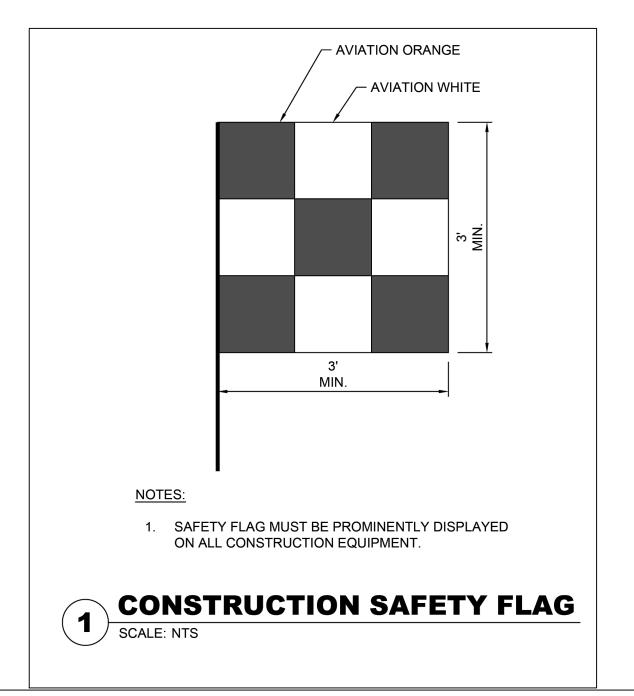
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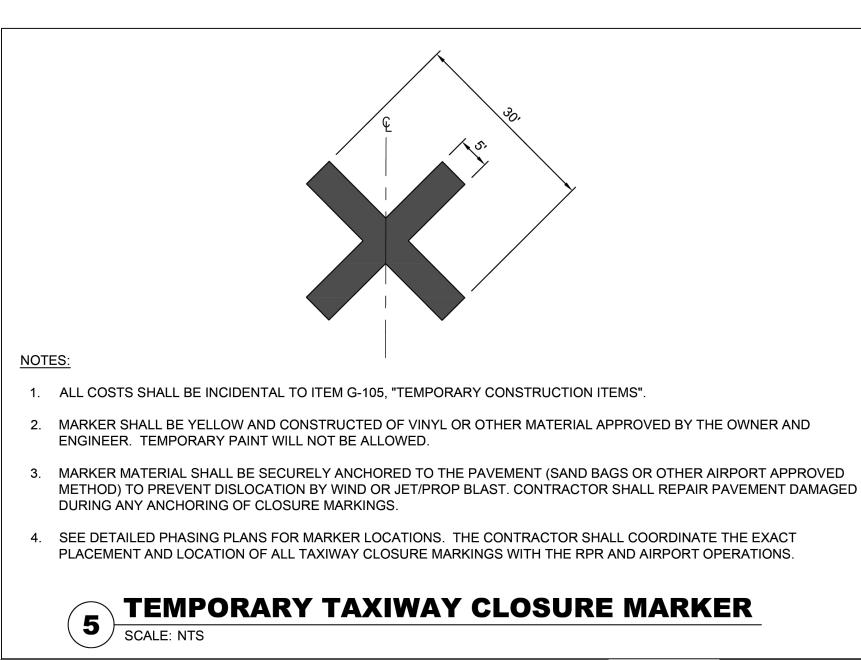
SHEET C-5.0 MARKING PLAN

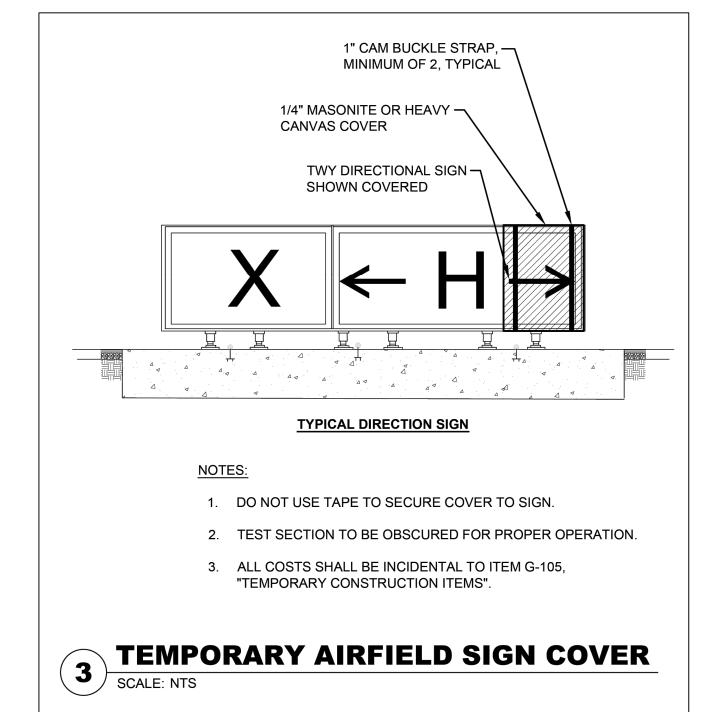
# TYPICAL SECTION & DETAILS

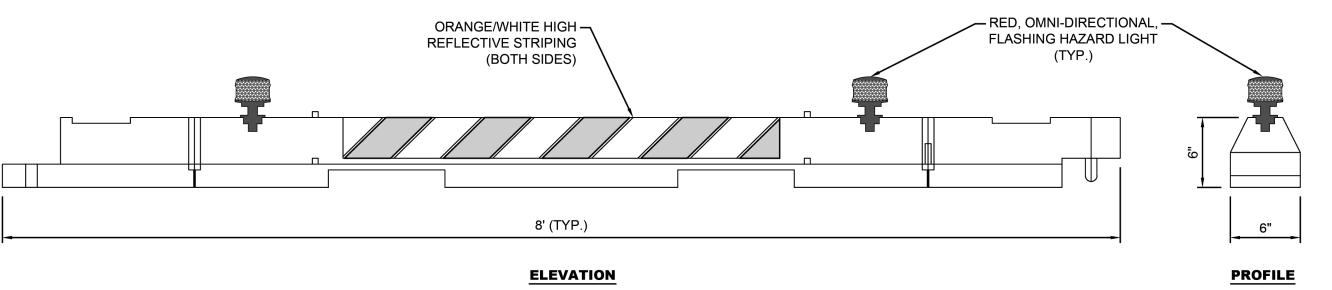


LEGEND	DESCRIPTION
1	REQ'D 3" OF P-403 ASPHALT SURFACE MIX
2	REQ'D 5" OF P-401 ASPHALT BINDER
A	EX. ASPHALT TO BE REMOVED (8" THICK)
В	EX. ASPHALT PAVEMENT (RET)
С	EX. CRUSHED AGGREGATE BASE COURSE (RET)
D	EXISTING SUBGRADE (RET)





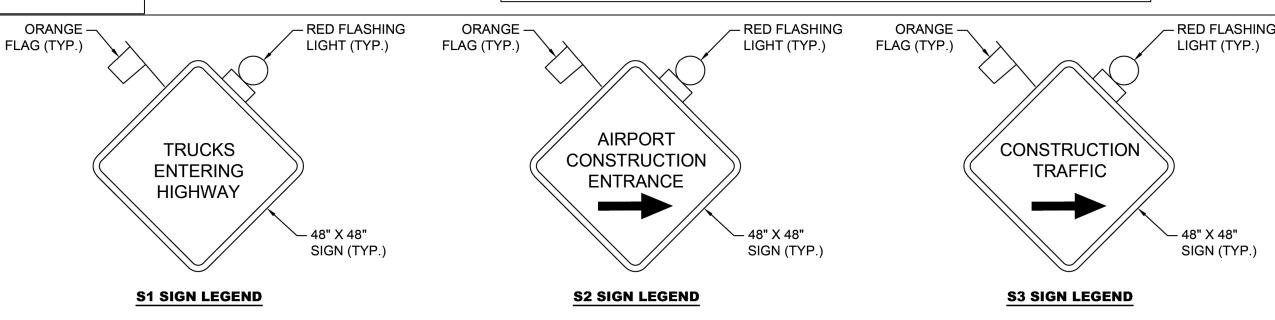




#### NOTES

- 1. THE CONTRACTOR MUST FURNISH THE BARRICADES AND CONTINUOUSLY MAINTAIN THE PLACEMENT, LOCATION, AND OPERATION OF THE LIGHTS FOR THE DURATION OF THE PROJECT. BARRICADES MUST BE INSPECTED DAILY BY THE CONTRACTOR AND THE RPR AND ANY DEFICIENCIES FOUND MUST BE CORRECTED IMMEDIATELY.
- 2. BARRICADES MUST BE PLACED WHERE NOTED IN THE PLANS, AROUND ALL PAVEMENT DROP-OFFS AND OPEN EXCAVATIONS, AND WHERE DIRECTED BY THE OWNER AND/OR THEIR REPRESENTATIVE.
- 3. PLACE BARRICADES INTERLOCKED ON PAVED SURFACES AND A MAXIMUM OF 50 FT. APART IN TURF AREAS UNLESS OTHERWISE NOTED. BARRICADES SHOULD ALTERNATE COLORS, ORANGE WHITE, AND INCLUDE RED, OMNI-DIRECTIONAL, FLASHING LIGHTS.
- 4. BARRICADES SHALL BE WEIGHTED BY FILLING WITH WATER OR USING SAND BAGS TO PREVENT MOVEMENT FROM HIGH WINDS AND JET OR PROP BLAST. WATER AND/OR SAND BAGS MUST BE OBTAINED FROM OFF-SITE SOURCE. OWNER WILL NOT PROVIDE WATER NOR SAND TO SECURE BARRICADES.
- 5. BARRICADES SHALL BE AR-10x96 LOW-PROFILE BARRICADES (MANUFACTURED BY MULTI-BARRIER), OR APPROVED EQUAL.
- 6. ALL COSTS SHALL BE INCIDENTAL TO ITEM G-105, "TEMPORARY CONSTRUCTION ITEMS".





#### NOTES

- 1. CONTRACTOR MUST FURNISH, INSTALL, AND MAINTAIN TEMPORARY CONSTRUCTION TRAFFIC SIGNS AS INDICATED ON THE PLANS.
- 2. S1 SIGNS MUST BE LOCATED A MIN. 300 FEET IN EACH DIRECTION FROM THE INTERSECTION WITH THE CONSTRUCTION ENTRANCE.
- 3. SEE SHEETS XXXX FOR SIGN LOCATIONS. THE CONTRACTOR SHALL COORDINATE THE EXACT PLACEMENT AND LOCATION OF ALL CONSTRUCTION TRAFFIC SIGNS WITH THE RPR AND AIRPORT OPERATIONS.
- 4. SIGN LETTERS ARE TO BE BLACK, MINIMUM HEIGHT OF 3", AND BE A BOLD LETTERING STYLE.
- 5. SIGN MOUNTING PER ALDOT STANDARD SPECIFICATIONS.
- 6. ARROW DIRECTIONS ON SIGNS MAY VARY.
- 7. ALL COSTS SHALL BE INCIDENTAL TO ITEM G-105, "TEMPORARY CONSTRUCTION ITEMS"





7	WGINEER CO.						
DATE	İ	3/27/2025	4/10/2025				
DESCRIPTION	NOT FOR CONSTRUCTION	ISSUED FOR REVIEW / COMMENT	FINAL DESIGN				

DESIGNED BY: PROJECT NUMBER: MARK

JH 25-E-01-03100

DRAWN BY: ISSUE DATE:

DJB --/--/2025

SCALE: NT C

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TYPICAL SECTION
& DETAIL SHEET