



REQUEST FOR PROPOSAL (RFP)
ADDENDUM NUMBER ONE (1)
DATE: July 2, 2025

PROJECT: Flight Information Display System
Birmingham-Shuttlesworth International Airport
Birmingham, Alabama

From: Birmingham Airport Authority (BAA)
5900 Messer Airport Highway
Birmingham, Alabama 35212

To: All Participants

General:

This addendum will form a part of the RFP and modifies the original Request for Proposal (RFP) document. The following changes take precedence over items in the RFP. Any portion of the RFP not changed by this Addendum remains in effect. Recipients of the Addendum are advised to provide this Addendum to anyone to whom they further distribute without the BAA's knowledge.

Participants in this RFP are required to acknowledge receipt of this Addendum in their proposal. Failure to do so may subject Proposer for disqualification.

ADDITIONAL INFORMATION/CLARIFICATIONS/DESCRIBED BELOW:

Due to the holiday weekend, the proposal due date is extended to July 15th at 2:00PM.

1. **Question:** We would like to check if we can bid only for installation & maintenance of FIDS software/application alone without supply of hardware's? Or are you looking for a vendor who can take care both FIDS Software & Hardware together and like to see quote for the same?

Answer: While our preference is to work with a vendor capable of providing both the software and hardware components of the FIDS solution, we remain open to alternative approaches. If your proposal focuses solely on the software aspect, please clearly outline how that model would work—particularly in terms of integration, support, and coordination with hardware providers. The Birmingham Airport Authority will give serious consideration to well-defined proposals that address only the software portion of the project.

2. **Question:** If the Prime Contractor is an MBE/DBE, will they get full credit for the MBE/DBE purchasing goals?

Answer: The RFP has an MBE/WBE requirement. If the prime contractor is an MBE or WBE, they will get full credit.

3. **Question:** The RFP was published 6/16/25 and is due 7/7/20. That gives proposers only 13 business days to research, develop and assemble a proposal. Also given the Minority Business goals and Good Faith Effort requirements, there is very little time to develop an inclusive and responsive team. Will the airport extend the due date by at least 2 weeks?

Answer: The BAA will be extending the due date to July 15 at 2:00PM.

4. **Question:** The Cost Proposal requires an alternate cost to replace the monitors. Please provide a detailed list of quantities, locations and sizes of monitors to be replaced. Additionally, please stipulate if there are installation locations that require a lift or special equipment.

Answer:

Standing FIDS: 24 - 43" Monitors

Hanging FIDS: 15 - 40" Monitors that require a lift

Hanging FIDS: 13 - 40" Monitors that require a ladder

GIDS: 19 - 40" Monitors that require a ladder

Desktop Locations: 4 – Standard Desktop Monitor

5. **Question:** Please provide server details and specifications.

Answer: If your proposal includes server requirements, we expect you to outline the recommended specifications as part of your submission.

6. **Question:** Please provide specifications of existing display controllers.

Answer: All existing display controllers are expected to be replaced as part of this project, making their specifications less relevant to the proposal. With that said, the current setup includes a mix of GMC7 units, Dell mini and micro-PCs, and a few Intel NUCs. These are connected to displays via HDMI or HDMI adapter cables.

7. **Question:** I'm curious to know if the public announcement system and the FIDS at Birmingham Shuttlesworth International Airport will be integrated into an airport operation center (AOC)?

Answer: The AOC will have access to both the PA and FIDS systems; however, full integration between these systems has not yet been discussed or defined. If your proposal includes integration with the AOC, please outline the approach and benefits, and it will be given full consideration.

- 8. Question:** What operating system are your display controllers currently running?
Answer: All existing display controllers are expected to be replaced as part of this project, making their specifications less relevant to the proposal, but they are imaged with a Linux OS.
- 9. Question:** For dual-screen setups, do both screens run independently or share synchronized content? “Display controllers” (thin client, OPS module, NUC, micro-computer) need more clarification.
Answer: Both monitors are in a dual-screen setup--synchronized content. A media player or display controller is required to render the FIDS content, and we are open to various solutions for this. However, replacing existing monitors with smart monitors may exceed the financial scope of this project.
- 10. Question:** Need more specific explanation on how Visual paging is needed, should it appear as scrolling ticker, full-screen text, overlay, or combination?
Answer: We prefer combination with the ability to take over a screen for full-screen text during emergencies, but minimum of scrolling ticker
- 11. Question:** Should emergency messages be triggered manually from admin panel or via an external API/system integration?
Answer: Admin Portal
- 12. Question:** For Baggage information, is that in the OAG feed or updated manually/vendor-side? And what statistics are expected?
Answer: For established, recurring flights, baggage information can be pulled from the OAG feed. However, the selected vendor’s system must also allow for manual input, updates, and modifications of all flight information, including baggage details. Additionally, the system should support basic reporting and statistical analysis. Please provide details on the reporting capabilities of your system, including what types of reports are available, how data is accessed, and whether custom reporting is supported.
- 13. Question:** Are vendors expected to update flight data directly in OAG, or will they make real-time updates within the FIDS portal we provide?
Answer: Vendors will utilize the FIDS portal we provide.
- 14. Question:** We would like to check if we can bid only for installation & maintenance of the FIDS software/application alone without the supply of hardware – or are you looking for a vendor who can provide both FIDS Software & Hardware together and expect a quote for the full scope?
Answer: While our preference is to work with a vendor capable of providing both the software and hardware components of the FIDS solution, we remain open to alternative approaches. If your proposal focuses solely on the software aspect, please clearly outline how that model would work—particularly in terms of integration, support, and coordination with hardware providers. The Birmingham Airport Authority will give serious consideration to well-defined proposals that address only the software portion of the project.
- 15. Question:** Do any of the current FIDS, GIDS, BIDS, or TBIDS screen locations require a lift for access during installation or maintenance?
Answer: Yes, about 12 screens will require a lift for access

16. Question: Alternatively, are all player locations accessible on foot or with a standard 6-foot ladder?

Answer: Currently, all players are integrated with the monitors. Therefore, a lift will be required to access the hanging FIDS displays. All other players are reachable on foot.

17. Question: Can BAA provide photos of the current TBIDS units, including: The device/player itself, Mounting hardware and placement, General surrounding environment (e.g., enclosure, exposure to elements).

Answer: The TBIDS are exposed to extreme temperatures but are protected from wind, rain, and snow. Please see photo below for reference.

18. Question: Are the TBIDS units currently interfaced with the baggage belt system?

Answer: No, the tug driver manually starts the baggage belt and can manually choose "First Bag". However, we do have roll-on/roll-off rules in place for the BIDS, if the driver does not select "First Bag" or "Last Bag," the flight will still automatically roll onto the assigned carousel BIDS based on its programming.

19. Question: For example, do they use relays or switches to prevent baggage belts from starting until an operator interacts with the TBIDS?

Answer: See question #18

20. Question: Are any general installation photos available to help vendors understand the current configuration of players, screens, mounts, and cabling?

Answer: Attached are photos of our main Hanging FIDS, Standing FIDS, and our GIDS. There are no photos of the inside of the Hanging FIDS but it is a large open area back behind the monitors and power and ethernet are located there as well. The GIDS power and ethernet are in the panel directly below the monitor with plenty of room. The GIDS panel slide up and off for access to player and power.

21. Question: How will amendments or addenda to the RFP be communicated to vendors?

Answer: The addendum will be solicited via email and post on the BAA website.

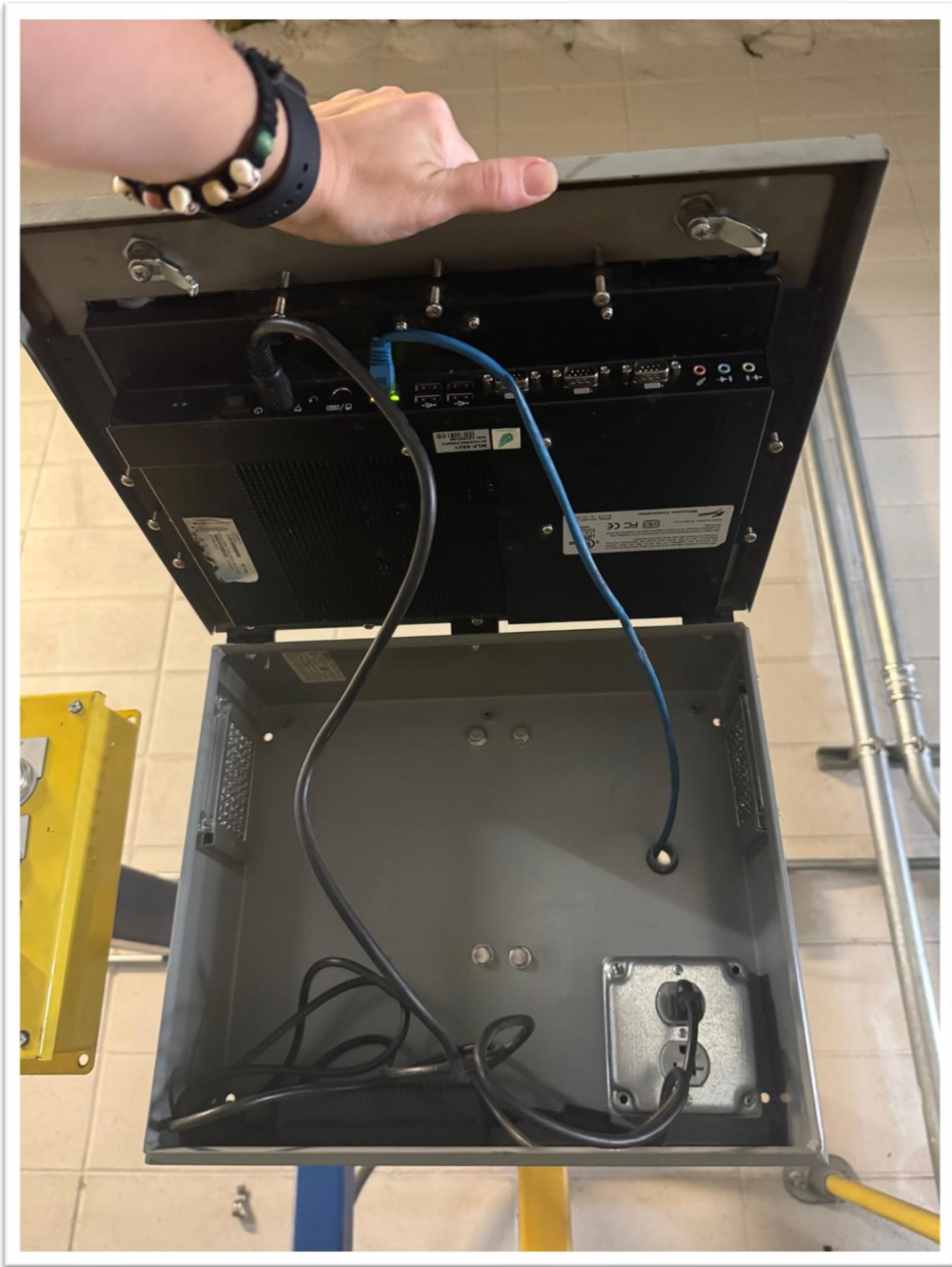
22. Question: Will updates be distributed via email to known respondents, or posted to a public website or procurement portal?

Answer: Updates will be via email and posted on the BAA website.

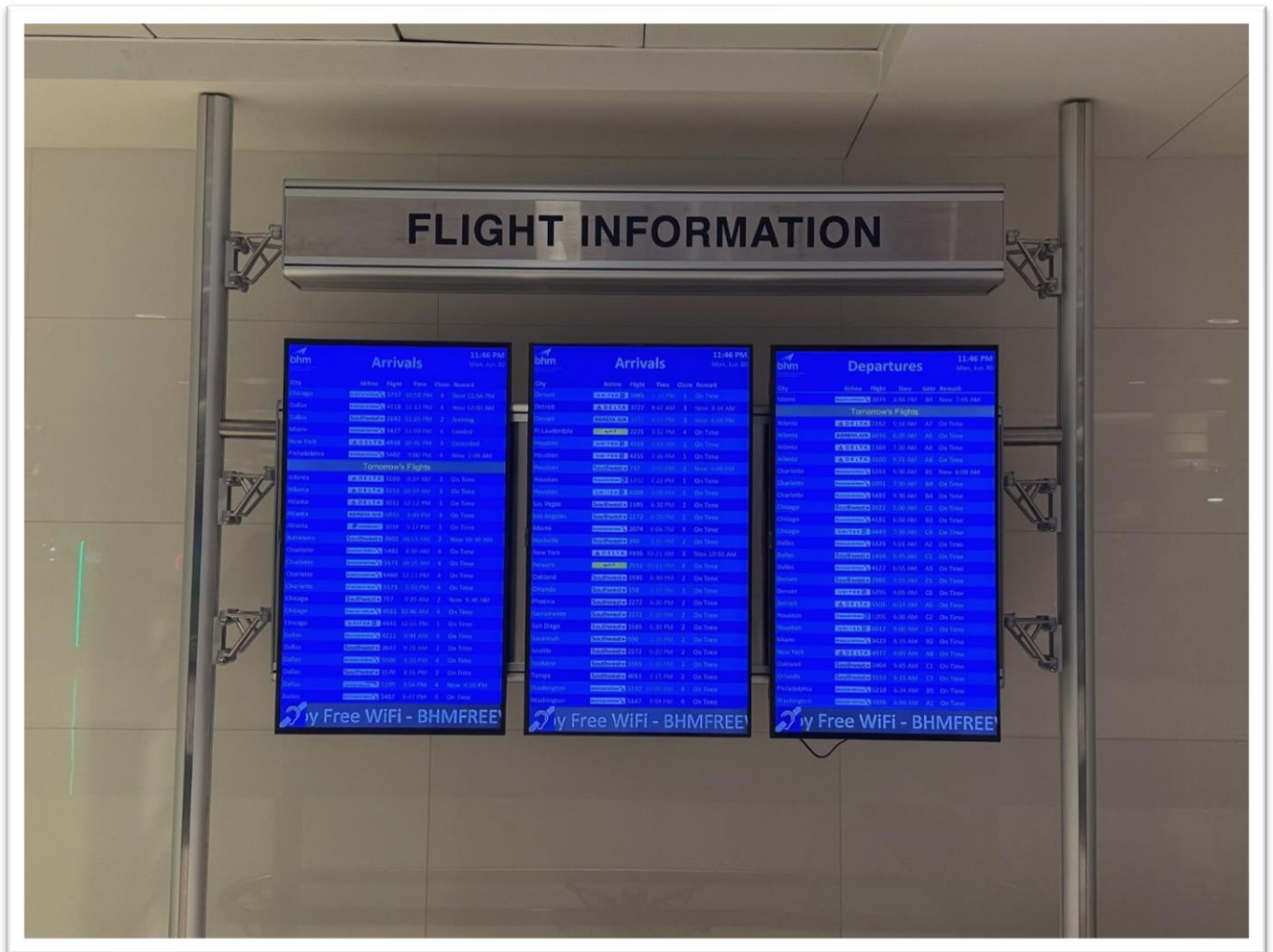
Reference to #17 – TBIDS Enclosure



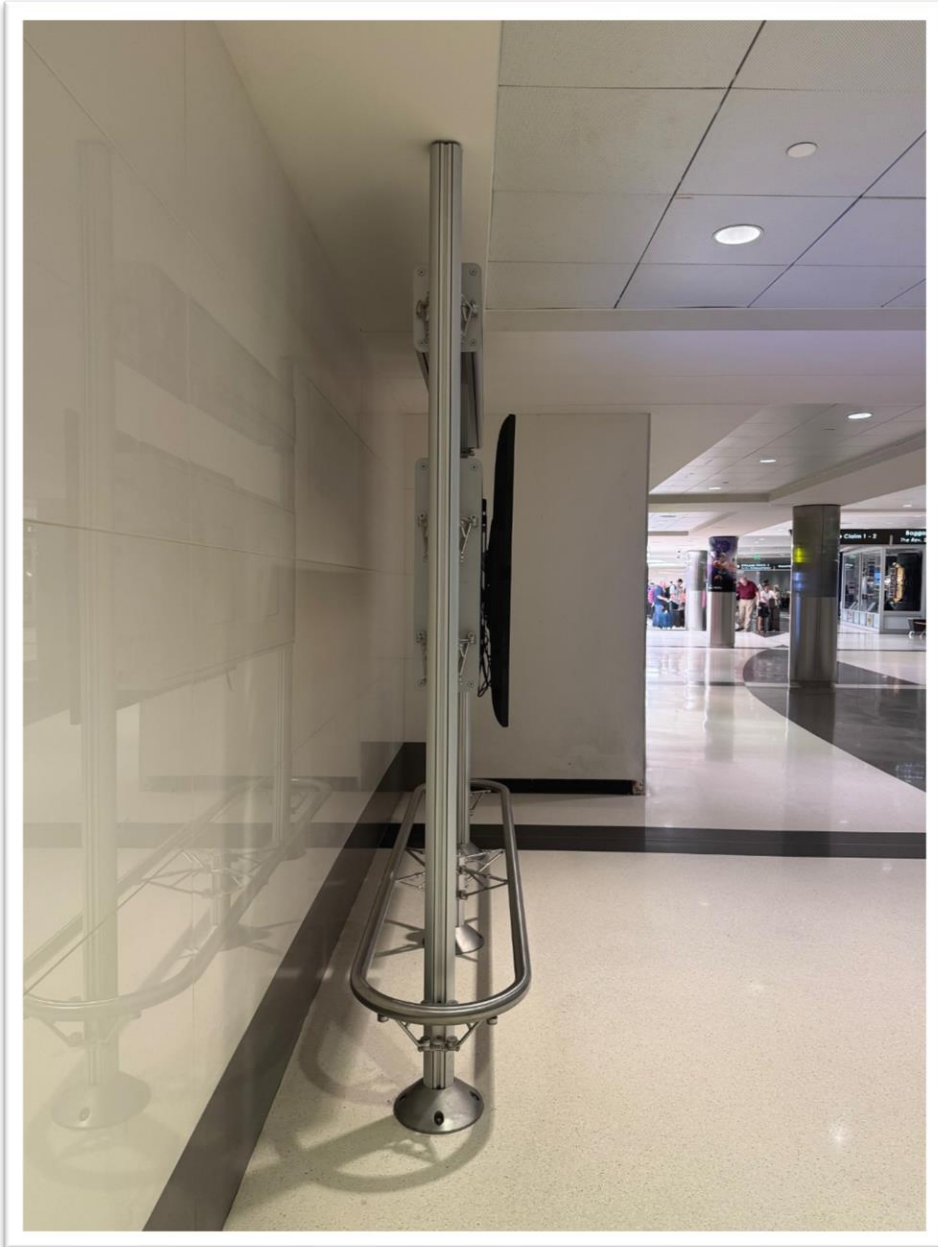
Reference to #17 - TBIDS Enclosure



Reference to # 20 – Standing FIDS Enclosure – Front view



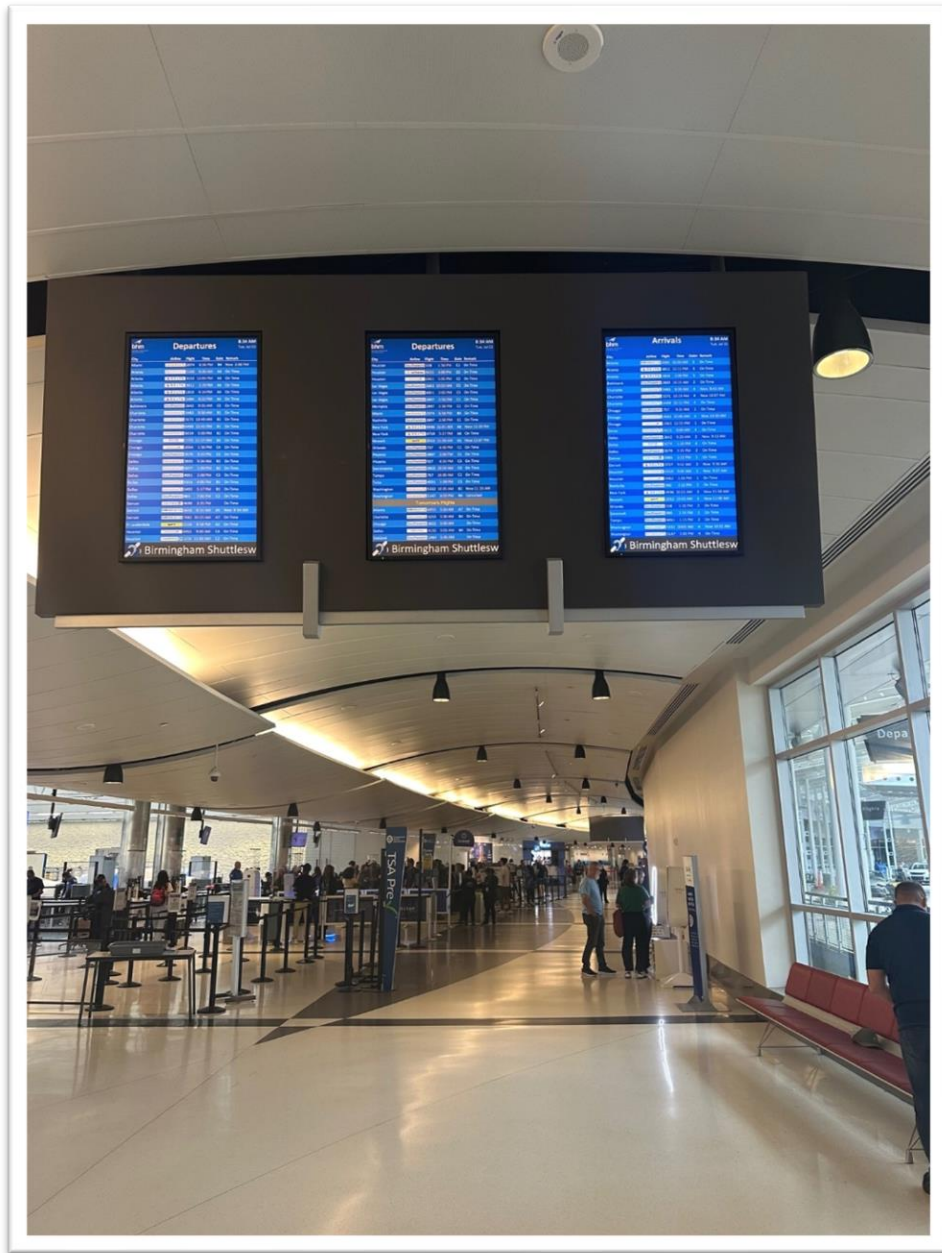
Reference to # 20 – Standing FIDS Enclosure – Side view



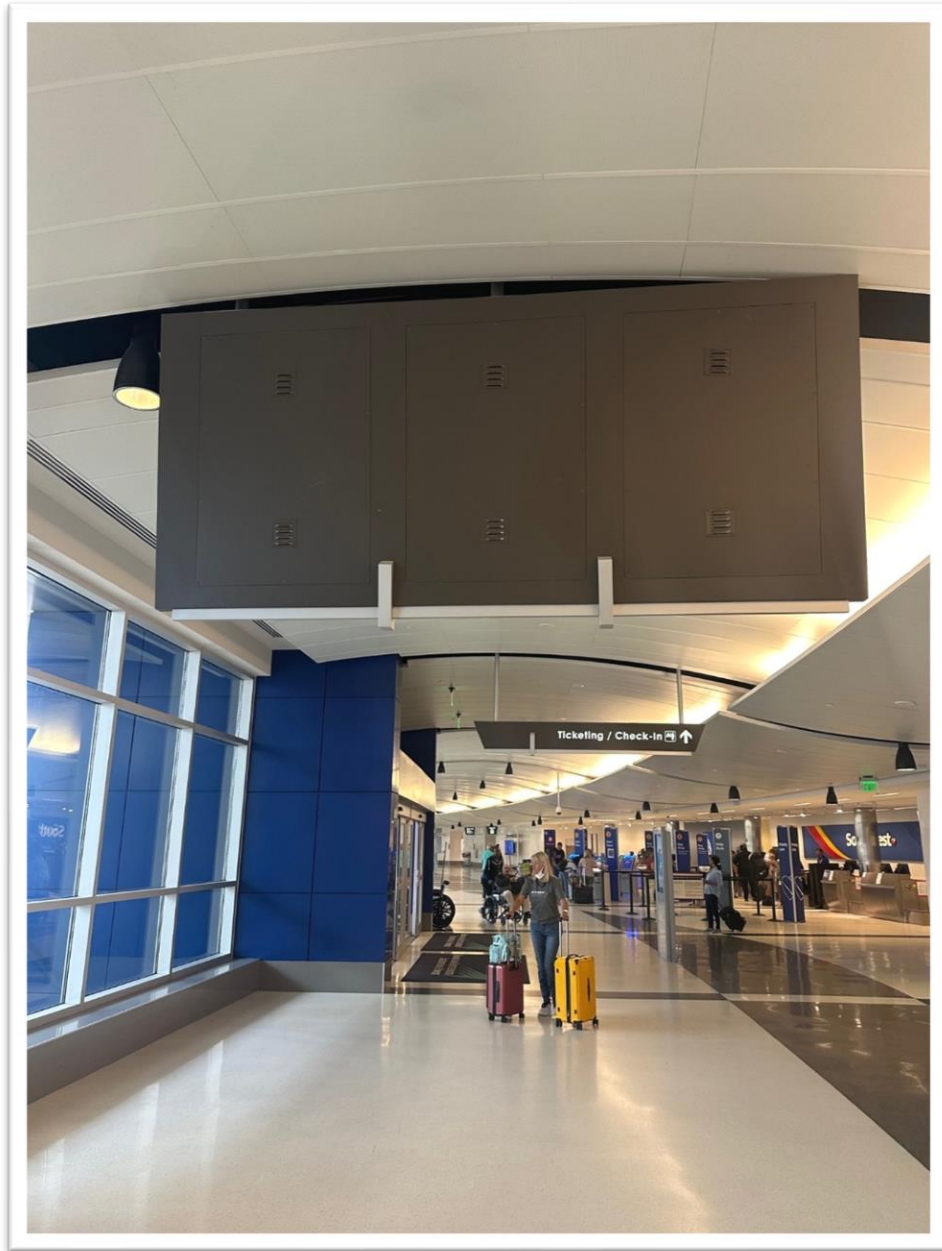
Reference to # 20 – Standing FIDS Enclosure – Inside



Reference to #20 – Hanging FIDS Enclosure – Front view



Reference to #20 – Hanging FIDS Enclosure – Back view



Reference to #20 - GIDS Enclosure – Front view

