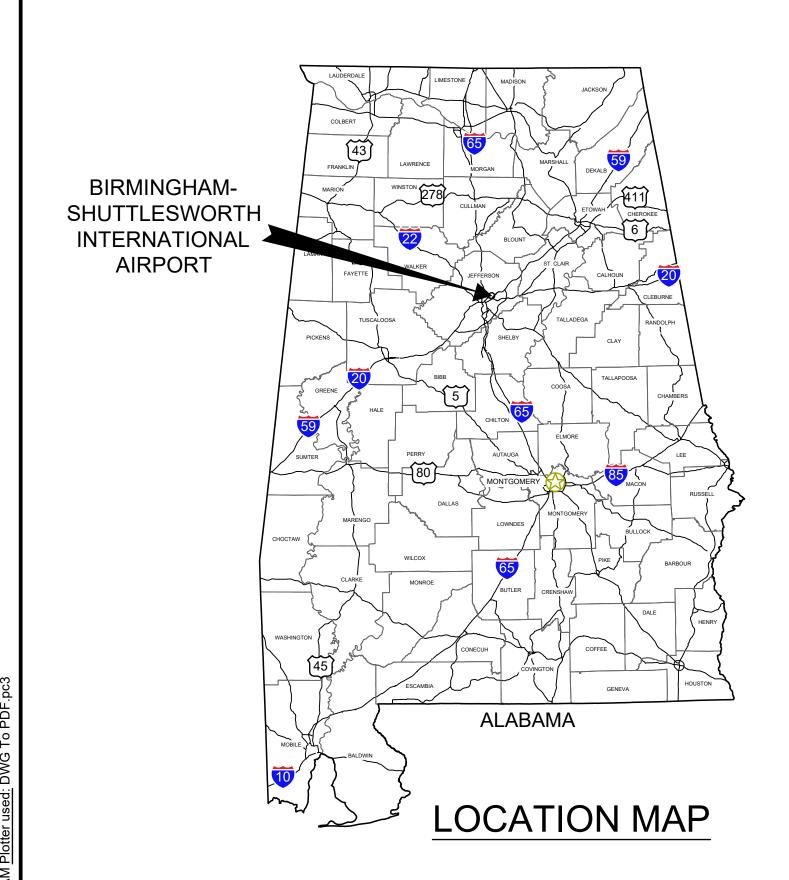
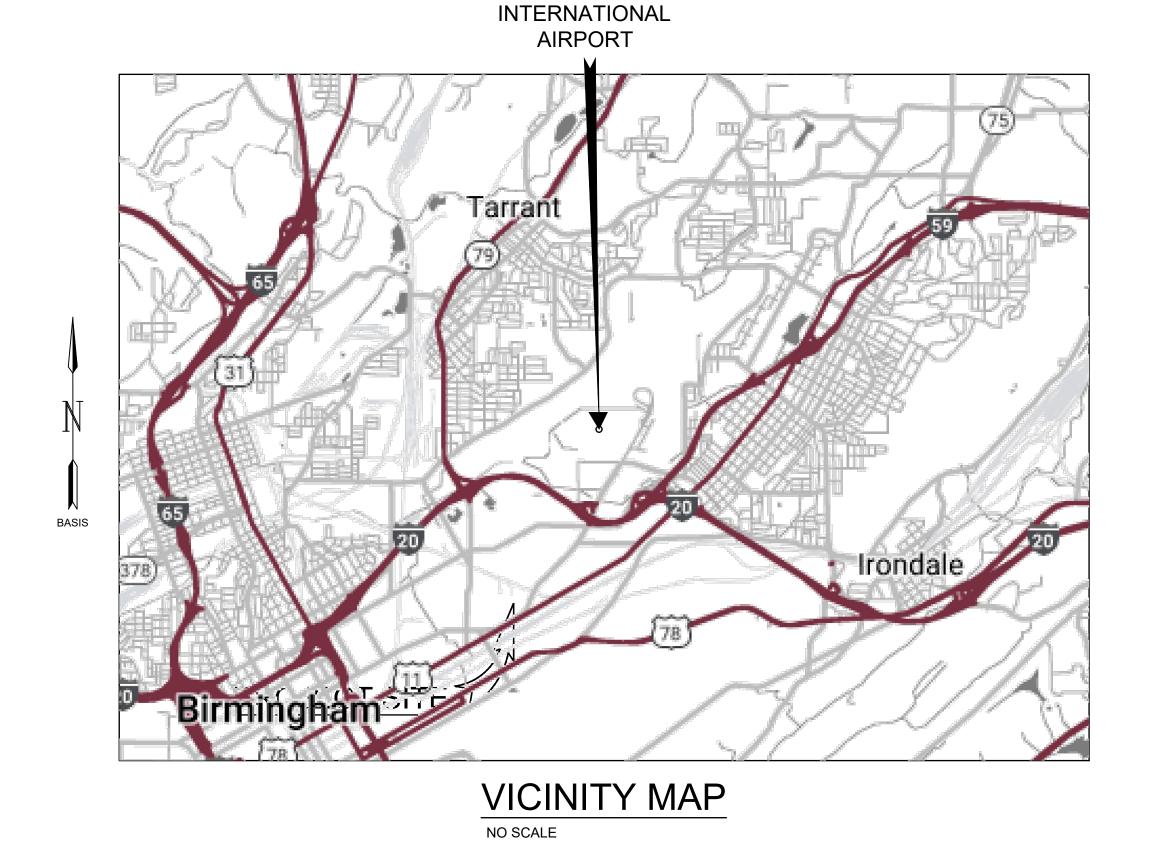
# EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT BIRMINGHAM, ALABAMA

**BIRMINGHAM-**

SHUTTLESWORTH





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GARVER PROJECT NO. 2302499 MAY 2025



PROFESSIONAL SERVICES

(256) 534-5512

AGREEMENT FOR THIS WORK

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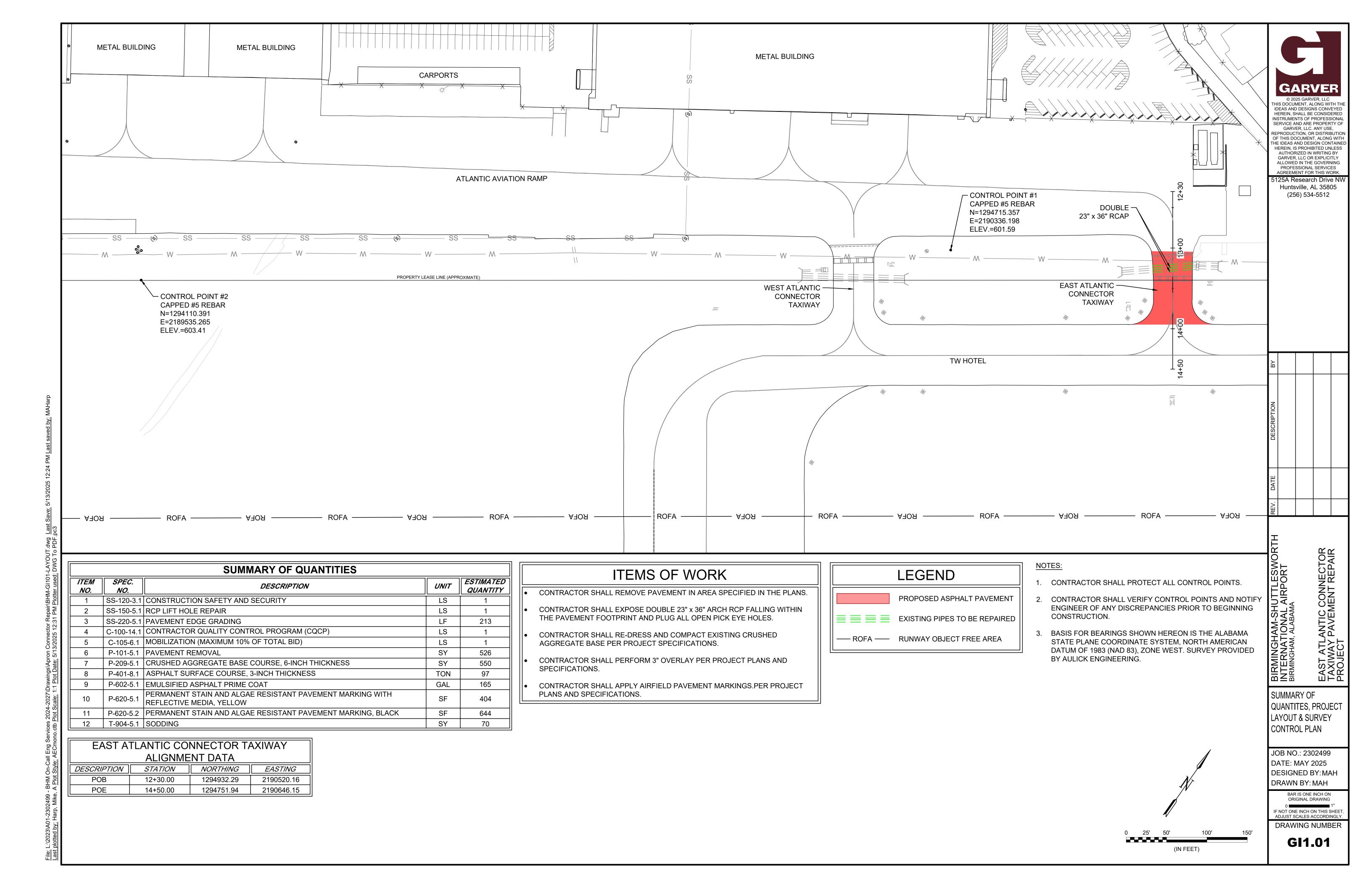
**COVER SHEET** 

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#### 1. COORDINATION

- .. CONTRACTOR PROGRESS MEETINGS THE OWNER, ENGINEER AND CONTRACTOR WILL HOLD PROGRESS MEETINGS ON A COORDINATED SCHEDULE DURING CONSTRUCTION. OPERATIONAL SAFETY WILL BE A STANDING AGENDA ITEM IN SUCH MEETINGS.
- B. SCOPE OR SCHEDULE CHANGES THE OWNER AND/OR ENGINEER WILL CALL SUCH COORDINATION CONFERENCES AS MAY SEEM EXPEDIENT TO HIM FOR THE PURPOSE OF ASSURING COORDINATION OF THE WORK COVERED BY THIS CONTRACT AND/OR SCOPE OR SCHEDULE CHANGES. THE CONTRACTOR SHALL ATTEND ALL SUCH CONFERENCES.
- C. FAA ATO COORDINATION COORDINATION WITH THE FAA ATO FOR THE REQUIRED SHUTDOWN OF FAA FACILITIES FOR THIS PROJECT WILL BE COORDINATED THROUGH THE ENGINEER AND AIRPORT OPERATIONS.

#### 2. PHASING

DURING PERFORMANCE OF THIS PROJECT, THE AIRPORT RUNWAYS, TAXIWAYS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE. THE PROJECT SHALL BE PHASED TO REDUCE OPERATIONAL IMPACTS AT THE AIRPORT.

- A. PHASE ELEMENTS IF NECESSARY FOR A GIVEN PHASE, EACH PHASE OF THE CONSTRUCTION SAFETY DRAWINGS SHALL DETAIL THE AREAS CLOSED TO AIRCRAFT OPERATIONS, ESTIMATED DURATION OF CLOSURES, CONSTRUCTION STAGING AREAS, AND CONSTRUCTION ACCESS AND HAUL ROUTES.
- B. CONSTRUCTION SAFETY DRAWINGS SEE SHEET CP1.01 FOR CONSTRUCTION SAFETY DRAWINGS.
- AREAS OF OPERATIONS AFFECTED BY CONSTRUCTION ACTIVITY
- A. IDENTIFICATION OF AFFECTED AREAS SEE CONSTRUCTION SAFETY DRAWINGS FOR AIRFIELD AREAS OF OPERATIONS AFFECTED BY CONSTRUCTION.
- B. MITIGATION EFFORTS SEE CONSTRUCTION SAFETY DRAWINGS FOR MITIGATION EFFORTS OF OPERATIONS AFFECTED BY CONSTRUCTION.
- 4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

THE CONTRACTOR MUST NOT CONDUCT ANY CONSTRUCTION ACTIVITY WITHIN NAVIGATIONAL AID RESTRICTED AREAS WITHOUT PRIOR APPROVAL FROM THE LOCAL FAA AIRWAY FACILITIES SECTOR REPRESENTATIVE. NAVIGATIONAL AIDS INCLUDE INSTRUMENT LANDING SYSTEM COMPONENTS, VERY HIGH-FREQUENCY OMNI-DIRECTIONAL RANGE STATIONS, AND AIRPORT SURVEILLANCE RADAR. SUCH RESTRICTED AREAS ARE DEPICTED ON CONSTRUCTION PLANS. PLANNED CONSTRUCTION ACTIVITIES WILL HAVE NO NEGATIVE IMPACTS ON THE FUNCTIONALITY AND SERVICEABILITY OF THE NAVAIDS.

- 5. CONTRACTOR ACCESS
  - A. LOCATION OF STOCKPILED MATERIALS THE CONTRACTOR SHALL INSTALL A TEMPORARY FENCE AROUND HIS CONSTRUCTION STAGING AREA TO SEPARATE HIS BATCH PLANT, MATERIAL STOCKPILE, EQUIPMENT STORAGE, AND PARKING AREAS FROM THE PUBLIC. NO PERSONAL VEHICLES OF CONTRACTOR'S EMPLOYEES WILL BE ALLOWED INSIDE THE SECURED AREA OF THE AIRPORT. ALL MATERIAL DELIVERIES SHALL BE RECEIVED IN THE STAGING AREA RESERVED BY THE CONTRACTOR. NO DELIVERY TRUCKS WILL BE ALLOWED ACCESS TO A SECURED AREA OF THE AIRPORT BEYOND THIS STAGING AREA. STOCKPILED MATERIALS AND EQUIPMENT ARE NOT PERMITTED WITHIN THE ACTIVE RUNWAY SAFETY AREA AND OBSTACLE FREE ZONE. THE CONTRACTOR SHALL RECEIVE APPROVAL FROM THE ENGINEER AND FAA AIR SPACING OFFICE PRIOR TO LOCATING STOCKPILES OR EQUIPMENT WITHIN THE OBJECT FREE AREA, SAFETY AREA, OR OBSTACLE FREE ZONE. NO STOCKPILE SHALL BE GREATER THAN 15-FT IN HEIGHT.
  - B. VEHICLE AND PEDESTRIAN OPERATIONS SEE THE CONSTRUCTION SAFETY DRAWINGS FOR CONSTRUCTION SITE PARKING, EQUIPMENT STORAGE AREAS, AND ACCESS AND HAUL ROUTES. VEHICULAR TRAFFIC SHALL ALWAYS YIELD TO AIRCRAFT TRAFFIC.

WHEN ANY VEHICLE, OTHER THAN ONE THAT HAS PRIOR APPROVAL FROM THE AIRPORT OPERATOR, MUST TRAVEL OVER ANY PORTION OF AN AIRCRAFT MOVEMENT AREA, IT WILL BE ESCORTED AND PROPERLY IDENTIFIED. TO OPERATE IN THOSE AREAS DURING DAYLIGHT HOURS, THE VEHICLE MUST HAVE A FLAG OR BEACON ATTACHED TO IT. ANY VEHICLE OPERATING ON THE MOVEMENT AREAS DURING HOURS OF DARKNESS OR REDUCED VISIBILITY MUST BE EQUIPPED WITH A FLASHING DOME-TYPE LIGHT, THE COLOR OF WHICH IS IN ACCORDANCE WITH LOCAL OR STATE CODES.

ALL CONSTRUCTION VEHICLES SHALL BE CLEARLY IDENTIFIED FOR CONTROL PURPOSES BY PROMINENTLY DISPLAYING THE COMPANY NAME ON EACH SIDE OF THE VEHICLE. THE IDENTIFICATION SYMBOLS SHOULD BE A MINIMUM 8-INCH BLOCK-TYPE CHARACTERS OF A CONTRASTING COLOR AND EASY TO READ. THEY MAY BE APPLIED EITHER BY USING TAPE OR A WATER-SOLUBLE PAINT TO FACILITATE REMOVAL. MAGNETIC SIGNS ARE ALSO ACCEPTABLE. IN ADDITION, VEHICLES MUST DISPLAY IDENTIFICATION MEDIA.

ALL VEHICLE OPERATORS HAVING ACCESS TO THE MOVEMENT AREA MUST UNDERGO BAA AIRFIELD DRIVER TRAINING TO BE FAMILIAR WITH AIRPORT PROCEDURES FOR THE OPERATION OF GROUND VEHICLES AND THE CONSEQUENCES OF NONCOMPLIANCE OR BE ESCORTED BY SOMEONE WHO IS.

PERSONNEL ENTERING THE SECURED AREA MUST BE IN POSSESSION OF AND DISPLAY A VALID AIRPORT IDENTIFICATION BADGE AT ALL TIMES OR MUST BE ESCORTED BY A PERSON WITH A VALID AIRPORT IDENTIFICATION BADGE. ANY PERSON WHO IS ESCORTING INDIVIDUALS MUST BE IN DIRECT CONTROL OF THE ESCORTED INDIVIDUALS AT ALL TIMES. ANY PERSON WHO HAS BEEN ISSUED A BADGE, BUT IS NOT IN POSSESSION OF THE BADGE, MAY NOT ENTER THE SECURED AREA OF THE AIRPORT. AIRPORT IDENTIFICATION BADGES MAY BE OBTAINED AT THE BHM BADGING OFFICE BY APPOINTMENT ONLY. THERE IS A \$25.00 CHARGE FOR ISSUANCE OF THE BADGE AND \$35.00 CHARGE FOR THE FINGERPRINTING AND SECURITY THREAT ASSESSMENT. ALL BADGES MUST BE RETURNED TO THE AIRPORT UPON COMPLETION OF THE PROJECT UNLESS DIRECTED OTHERWISE BY THE AIRPORT. ANY FINE, INCLUDING ANY AND ALL ASSOCIATED COSTS, ASSESSED THE AIRPORT FOR FAILURE TO MAINTAIN SECURITY OF THE AIRPORT WHICH ARE A RESULT OF THE NEGLIGENCE OF THE PRIME CONTRACTOR, ANY OF HIS/HER SUBCONTRACTORS, OR ANY SUPPLY/DELIVERY PERSONNEL, WILL BE ASSESSED TO THE PRIME CONTRACTOR AND SHALL BE DEDUCTED FROM ANY MONIES DUE HIM.

VEHICULAR TRAFFIC LOCATED IN OR CROSSING AN ACTIVE MOVEMENT AREA MUST HAVE A WORKING TWO-WAY RADIO IN CONTACT WITH THE CONTROL TOWER OR BE ESCORTED BY A PERSON IN RADIO CONTACT WITH THE TOWER. THE DRIVER, THROUGH PERSONAL OBSERVATION, SHOULD CONFIRM THAT NO AIRCRAFT IS APPROACHING THE VEHICLE POSITION. CONSTRUCTION PERSONNEL MAY OPERATE IN A MOVEMENT AREA WITHOUT TWO-WAY RADIO COMMUNICATION PROVIDED A NOTAM IS ISSUED CLOSING THE AREA AND THE AREA IS PROPERLY MARKED TO PREVENT INCURSIONS. ]

- C. CONTROL OF GATES THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SECURITY OF THE ACCESS GATES BY KEEPING THE ACCESS GATE LOCKED OR GUARDED AT ALL TIMES. SHOULD THE CONTRACTOR FAIL, AT ANY TIME, TO KEEP THE ACCESS GATE LOCKED OR GUARDED, THERE SHALL BE A FINE OF \$200.00 ASSESSED TO THE CONTRACTOR, FOR EACH OCCURRENCE THAT THE CONTRACTOR FAILS TO MAINTAIN THE SECURITY OF THE ACCESS GATE. ALL FINES ASSESSED TO THE CONTRACTOR SHALL BE DEDUCTED FROM ANY MONIES DUE TO HIM/HER.
- 6. WILDLIFE MANAGEMENT

IF APPLICABLE, THE CONTRACTOR SHALL REVIEW AND ADHERE TO THE CONTENTS OF THE AIRPORT OPERATOR'S WILDLIFE HAZARD MANAGEMENT PLAN. THE CONTRACTOR SHALL ALSO REVIEW AC 150/5200-33, HAZARDOUS WILDLIFE ATTRACTANTS ON OR NEAR AIRPORTS, AND CERTALERT 98-05, GRASSES ATTRACTIVE TO HAZARDOUS WILDLIFE (www.faa.gov). THE CONTRACTOR SHALL CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT MIGHT ATTRACT WILDLIFE. CONTRACTOR PERSONNEL MUST BE AWARE OF AND AVOID CONSTRUCTION ACTIVITIES THAT CAN CREATE WILDLIFE HAZARDS ON AIRPORTS. THE CONTRACTOR SHALL MITIGATE THE FOLLOWING ITEMS.

- A. TRASH THE CONTRACTOR SHALL PERFORM TRASH CLEAN-UP ON A DAILY BASIS.
- B. STANDING WATER THE CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE DURING CONSTRUCTION TO AVOID STANDING WATER.
- C. TALL GRASS AND SEEDS THE CONTRACTOR SHALL ADHERE TO THE REQUIREMENTS OF SECTION T-901, SEEDING OF THE CONTRACT DOCUMENTS AND SPECIFICATIONS.
- D. POORLY MAINTAINED FENCING AND GATES THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DAMAGE TO GATES OR FENCES. THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRS TO ANY GATES OR FENCES CAUSED BY NEGLIGENCE BY THE CONTRACTOR.

E. DISRUPTION OF EXISTING WILDLIFE HABITAT - THE CONTRACTOR SHALL NOTIFY THE AIRPORT IMMEDIATELY OF ANY WILDLIFE SIGHTINGS.

#### 7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

THE CONTRACTOR SHALL ENSURE THAT THE PAVEMENT SURFACES ARE KEPT CLEAN FROM DIRT, MUD, AND OTHER DEBRIS FROM THE CONTRACTOR'S EQUIPMENT. FREQUENT CLEAN UP IN THE VICINITY OF CONTRACTOR'S WORK AREAS IS REQUIRED. SEE AC 150/5210-24, FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT (www.faa.gov) FOR FURTHER INSTRUCTION.

# 8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

IF ANY CONSTRUCTION VEHICLE OR EQUIPMENT IS OPERATED WITHIN AIRPORT PROPERTY, THE CONTRACTOR MUST BE ADEQUATELY PREPARED TO EXPEDITIOUSLY CONTAIN AND CLEAN-UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS. SPECIAL CARE MUST ALSO BE TAKEN WHEN HANDLING OR TRANSPORTING HAZARDOUS MATERIALS ON AIRPORT PROPERTY. SEE AC 150/5320-15, MANAGEMENT OF AIRPORT INDUSTRIAL WASTE (www.faa.gov), FOR FURTHER INSTRUCTION.

## . NOTIFICATION OF CONSTRUCTION ACTIVITIES

- A. LIST OF RESPONSIBLE REPRESENTATIVES A POINT OF CONTACT LIST WILL BE COMPLETED AS PART OF THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) AND WILL BE DELIVERED TO ALL PARTIES PRIOR TO CONSTRUCTION.
- 3. NOTICES TO AIR MISSIONS (NOTAM) BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR MUST, THROUGH THE AIRPORT OPERATOR, GIVE NOTICE USING THE NOTAM SYSTEM OF PROPOSED LOCATION, TIME, AND DATE OF COMMENCEMENT OF CONSTRUCTION. UPON COMPLETION OF WORK AND RETURN OF ALL SUCH AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST, THROUGH THE AIRPORT OPERATOR, VERIFY THE CANCELLATION OF ALL NOTICES ISSUED VIA THE NOTAM SYSTEM.
- C. EMERGENCY NOTIFICATION PROCEDURES IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911, THEN NOTIFY THE ENGINEER AND AIRPORT OPERATIONS.
- D. COORDINATION WITH ARFF PERSONNEL ANY DEACTIVATION OF WATER LINES OR HYDRANTS, REROUTING OF ACCESS ROUTES, OR USE OF HAZARDOUS MATERIALS ON THE AIRFIELD SHALL BE COORDINATED AND APPROVED BY THE AIRPORT'S ARFF PERSONNEL PRIOR TO EXECUTION OF SUCH ACTIVITIES.
- E. NOTIFICATION TO THE FAA THE CONTRACTOR SHALL ENSURE THAT ALL CONSTRUCTION EQUIPMENT IS AIR SPACED THROUGH THE APPROPRIATE FAA REGIONAL OR DISTRICT OFFICE PRIOR TO USING SUCH EQUIPMENT ON SITE.
- F. SHUTDOWN OF ANY NAVAID (AIRPORT OR FAA OWNED) SHALL BE COORDINATED WITH THE FAA ATO 45 DAYS PRIOR TO THE PROPOSED SHUTDOWN.THE CONTRACTOR SHALL PROVIDE AN ADDITIONAL SEVEN DAYS ADVANCE NOTICE TO THE AIRPORT TO COORDINATE WITH THE FAA ATO TECH OPS OFFICE RESPONSIBLE FOR THE FAA FACILITIES. SHUTDOWN OF AN AIRPORT OWNED AND FAA MAINTAINED NAVAID OF 24 HOURS OR GREATER, OR MORE THAN 4 HOURS DAILY ON CONSECUTIVE DAYS, SHALL BE COORDINATED WITH THE FAA ATO A MINIMUM OF 45 DAYS PRIOR TO THE SHUTDOWN.

#### 10. INSPECTION REQUIREMENTS

- A. DAILY INSPECTIONS THE CONTRACTOR SHALL PERFORM DAILY SAFETY INSPECTIONS TO VERIFY ALL CONSTRUCTION OPERATIONS ARE IN CONFORMANCE WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
- 3. INTERIM INSPECTIONS PRIOR TO OPENING ANY PORTION OF THE AIRPORT TO TRAFFIC, THE CONTRACTOR, ENGINEER, AND AIRPORT OPERATOR SHALL PERFORM A SAFETY INSPECTION OF THE AREA TO BE OPENED TO TRAFFIC TO VERIFY CONFORMANCE WITH THE CSPP AND FAA STANDARDS.
- C. FINAL INSPECTIONS PRIOR TO OPENING ANY PORTION OF THE AIRPORT TO TRAFFIC, THE CONTRACTOR, ENGINEER, AND AIRPORT OPERATOR SHALL PERFORM A SAFETY INSPECTION OF THE AREA TO BE OPENED TO TRAFFIC TO VERIFY CONFORMANCE WITH THE CSPP AND FAA STANDARDS.

# 11. UNDERGROUND UTILITIES

UNDERGROUND UTILITIES EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS. HOWEVER, ALL EXISTING UTILITIES MAY NOT BE SHOWN AND THE ACTUAL LOCATIONS OF THE UTILITIES MAY VARY FROM THE LOCATIONS SHOWN. PRIOR TO BEGINNING ANY TYPE OF EXCAVATION, THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITIES ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY.

ALABAMA STATE LAW, THE UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, REQUIRES TWO WORKING DAYS ADVANCE NOTIFICATION THROUGH THE ONE-CALL SYSTEM CENTER BEFORE EXCAVATING USING MECHANIZED EQUIPMENT OR EXPLOSIVES (EXCEPT IN THE CASE OF AN EMERGENCY). THE ONE-CALL SYSTEM PHONE NUMBER IS 1-800-292-8525. THE CONTRACTOR IS ADVISED THAT THERE IS A SEVERE PENALTY FOR NOT MAKING THIS CALL. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE ALABAMA ONE-CALL SYSTEM; THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITIES AS WELL AS THE ONE-CALL SYSTEM.

# 12. PENALTIES

FAILURE OF THE CONTRACTOR (INCLUDING EMPLOYEES) OR ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) TO COMPLY WITH ATCT INSTRUCTIONS, THE AIRPORT SAFETY PLAN, OR ANY OF THE OTHER REQUIREMENTS OF THE AIRPORT WHILE OPERATING ON AIRPORT PROPERTY, SHALL BE SUBJECT TO THE FOLLOWING:

- A. FIRST OFFENSE CONTRACTOR SHALL RECEIVE A WRITTEN WARNING, AND THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINES OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.
- B. SECOND OFFENSE THE CONTRACTOR SHALL RECEIVE A FINE OF \$1,000.00 TO BE DEDUCTED FROM ANY MONIES DUE HIM, AND THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINE OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.
- C. THIRD OFFENSE WORK WILL BE SUSPENDED. THE CONTRACTOR (INCLUDING EMPLOYEES) AND ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) WHO WILL OPERATE GROUND VEHICLES ON THE AIRPORT SHALL SUCCESSFULLY COMPLETE, FOR A SECOND TIME, FORMALIZED AIRPORT SAFETY TRAINING, TO BE CONDUCTED BY AIRPORT STAFF. WHEN THE CONTRACTOR'S EMPLOYEES HAVE COMPLETED AIRPORT SAFETY TRAINING TO THE SATISFACTION OF THE OWNER, WORK MAY CONTINUE AT THE DISCRETION OF THE OWNER.

# 13. SPECIAL CONDITIONS

NONE

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BIRMINGHAM-SHUTTLESW(INTERNATIONAL AIRPORT BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTORY

TAXIWAY PAVEMENT REPA

CONSTRUCTION SAFETY & PHASING PLAN NOTES

JOB NO.: 2302499 DATE: MAY 2025 DESIGNED BY: MAH DRAWN BY: MAH

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#### 14. RUNWAY AND TAXIWAY VISUAL AIDS

- A. GENERAL ALL AIRPORT MARKINGS, LIGHTING, SIGNS, AND VISUAL NAVAIDS THAT ARE IN OPERATION MUST BE CLEAR FROM ALL OBSTRUCTIONS. ALL TEMPORARY MARKINGS, SIGNS, LIGHTS, OR OTHER VISUAL AIDS MUST BE SECURED IN PLACE TO PREVENT DAMAGE OR DISPLACEMENT BY PROP WASH, JET BLAST, WING VORTICES, OR OTHER WIND CURRENTS.
- B. MARKINGS ALL TEMPORARY OR PERMANENT RUNWAY AND TAXIWAY VISUAL AIDS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC 150/5340-1 (www.faa.gov). MARKINGS FOR THIS PROJECT INCLUDE THE FOLLOWING:
- TEMPORARILY CLOSED TAXIWAYS THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, AND MAINTAINING LOW PROFILE BARRICADES AT THE ENTRANCE TO THE CLOSED TAXIWAY FROM AN ADJACENT TAXIWAY. BARRICADES SHALL BE INSTALLED OUTSIDE ALL ACTIVE TAXIWAY SAFETY AREAS. SEE DETAILS ON CONSTRUCTION SAFETY DRAWING FOR LOW-PROFILE AIRCRAFT BARRICADE DETAIL.
- C. LIGHTING AND VISUAL NAVAIDS ALL TEMPORARY LIGHTING FOR RUNWAY AND TAXIWAY SYSTEMS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC 150/5340-30 AND 150/5345-50 (www.faa.gov). THE CONTRACTOR SHALL BE RESPONSIBLE FOR DISCONNECTING ISOLATION TRANSFORMERS ASSOCIATED WITH ANY RUNWAY OR TAXIWAY LIGHT FIXTURES THAT ARE BEING DISCONNECTED.

IF APPLICABLE, ALL CONSTRUCTION, ALTERATION, OR REMOVAL OF FAA OWNED EQUIPMENT SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS SET FORTH IN THE APPROVED FAA REIMBURSABLE AGREEMENT. NO WORK SHALL BE COMPLETED ON FAA OWNED EQUIPMENT PRIOR TO COMPLETION OF THE FAA REIMBURSABLE AGREEMENT.

- D. SIGNS THE CONTRACTOR SHALL INSTALL ALL SIGNS IN ACCORDANCE WITH THE MOST RECENT EDITION OF FAA AC 150/5345-44 AND 150/5340-18. ANY SIGN THAT IS NOT PERFORMING ITS NORMAL FUNCTION MUST BE COVERED OR REMOVED TO PREVENT MISLEADING PILOTS.
- 15. MARKING AND SIGNS FOR ACCESS ROUTES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING AND INSTALLING ALL NECESSARY MARKINGS AND SIGNAGE FOR ALL ACCESS ROUTES TO AND FROM THE SITE TO BE USED BY CONTRACTOR PERSONNEL, SUBCONTRACTOR PERSONNEL, OR DELIVERY OPERATIONS. ALL SIGNAGE IN THE AIR OPERATIONS AREA SHALL BE FRANGIBLY MOUNTED.

- 16. HAZARD MARKING AND LIGHTING
  - A. PURPOSE HAZARD MARKING AND LIGHTING PREVENTS PILOTS FROM ENTERING AREAS CLOSED TO AIRCRAFT AND PREVENTS CONTRACTOR PERSONNEL FROM ENTERING AREAS OPEN TO AIRCRAFT.
  - B. EQUIPMENT THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN LOW-PROFILE BARRICADES IN HAZARDOUS AREAS INSIDE MOVEMENT AREAS. BARRICADES SHALL RESTRICT ACCESS AND MAKE HAZARDS OBVIOUS TO AIRCRAFT, PERSONNEL, AND VEHICLES. DURING PERIODS OF LOW VISIBILITY AND AT NIGHT, BARRICADES SHALL BE EQUIPPED WITH RED FLASHING OR STEADY BURNING LIGHTS. THE SPACING OF BARRICADES SHALL BE SUCH THAT A BREACH IS PHYSICALLY PREVENTED BARRING A DELIBERATE ACT. IF BARRICADES ARE INTENDED TO PREVENT PEDESTRIANS, THEN THEY SHALL BE LINKED. SEE DETAILS ON CONSTRUCTION SAFETY DRAWINGS FOR LOW-PROFILE AIRCRAFT BARRICADE DETAIL.
- 17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

ALL WORK CONDUCTED AT NIGHT SHALL BE ACCOMPANIED BY ADEQUATE LIGHT FACILITIES TO COMPLETE THE WORK. ALL LIGHT FACILITIES SHALL BE AIMED OR SHIELDED AS NECESSARY TO AVOID IMPACTING AIRCRAFT OR ATCT OPERATIONS. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A LIGHTING PLAN SHOWING THE LOCATION AND AIMING DIRECTION OF ALL LIGHT FACILITIES PRIOR TO THE COMPLETION OF ANY NIGHT WORK.

- 18. PROTECTION OF SAFETY AREAS, OBJECT FREE AREAS, OBJECT FREE ZONES, AND APPROACH/DEPARTURE SURFACES.
  - A. RUNWAY SAFETY AREAS (RSA) NO WORK SHALL BE PERMITTED WITHIN AN ACTIVE RUNWAY SAFETY AREA.
  - B. RUNWAY OBJECT FREE AREAS (ROFA) NO MATERIAL SHALL BE STOCKPILED INSIDE THE LIMITS OF THE ACTIVE ROFA.
  - C. TAXIWAY SAFETY AREAS (TSA) NO WORK SHALL BE PERMITTED WITHIN AN ACTIVE TSA.
  - D. TAXIWAY OBJECT FREE AREAS (TOFA) NO CONSTRUCTION SHALL BE PERMITTED INSIDE AN ACTIVE TOFA.
  - E. OBSTACLE FREE ZONE (OFZ) NO PERSONNEL, MATERIAL, OR EQUIPMENT SHALL PENETRATE THE OFZ WHILE THE RUNWAY IS OPEN TO OPERATIONS. THE DIMENSIONS OF THE OFZ ARE AS DEFINED IN FAA AC 150/5300-13 (www.faa.gov).
  - F. APPROACH/DEPARTURE SURFACES ALL CONTRACTOR PERSONNEL, MATERIALS, AND EQUIPMENT SHALL REMAIN CLEAR OF THE APPLICABLE THRESHOLD SITING SURFACES AS DEFINED IN CHAPTER 3 OF FAA AC 150/5300-13 (www.faa.gov).
- 19. OTHER LIMITATIONS ON CONSTRUCTION
  - A. PROHIBITIONS THE USE OF TALL EQUIPMENT (I.E. CRANES, CONCRETE PUMPS) SHALL NOT BE PERMITTED UNLESS APPROVED BY THE ENGINEER.

OPEN FLAME WELDING AND TORCH CUTTING OPERATIONS ARE NOT PERMITTED UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED AND THESE OPERATIONS ARE AUTHORIZED BY THE AIRPORT OPERATOR AND THE ENGINEER.

ELECTRICAL BLASTING CAPS SHALL NOT BE PERMITTED WITHIN 1,000-FT OF THE AIRPORT PROPERTY. FLARE POTS ARE NOT PERMITTED WITHIN THE AIR OPERATIONS AREA.

B. RESTRICTIONS - NONE



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CONSTRUCTION
SAFETY & PHASING
PLAN NOTES

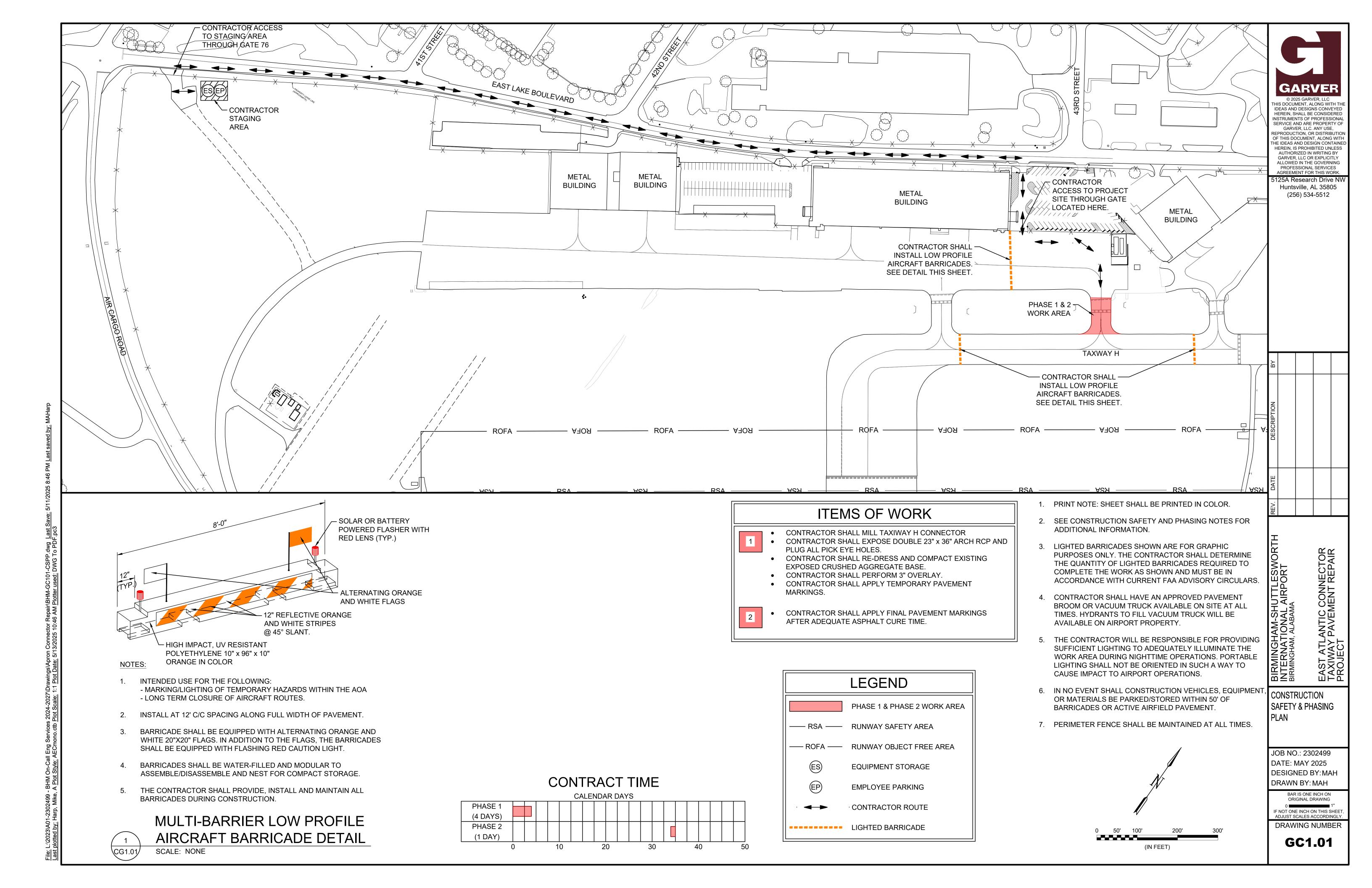
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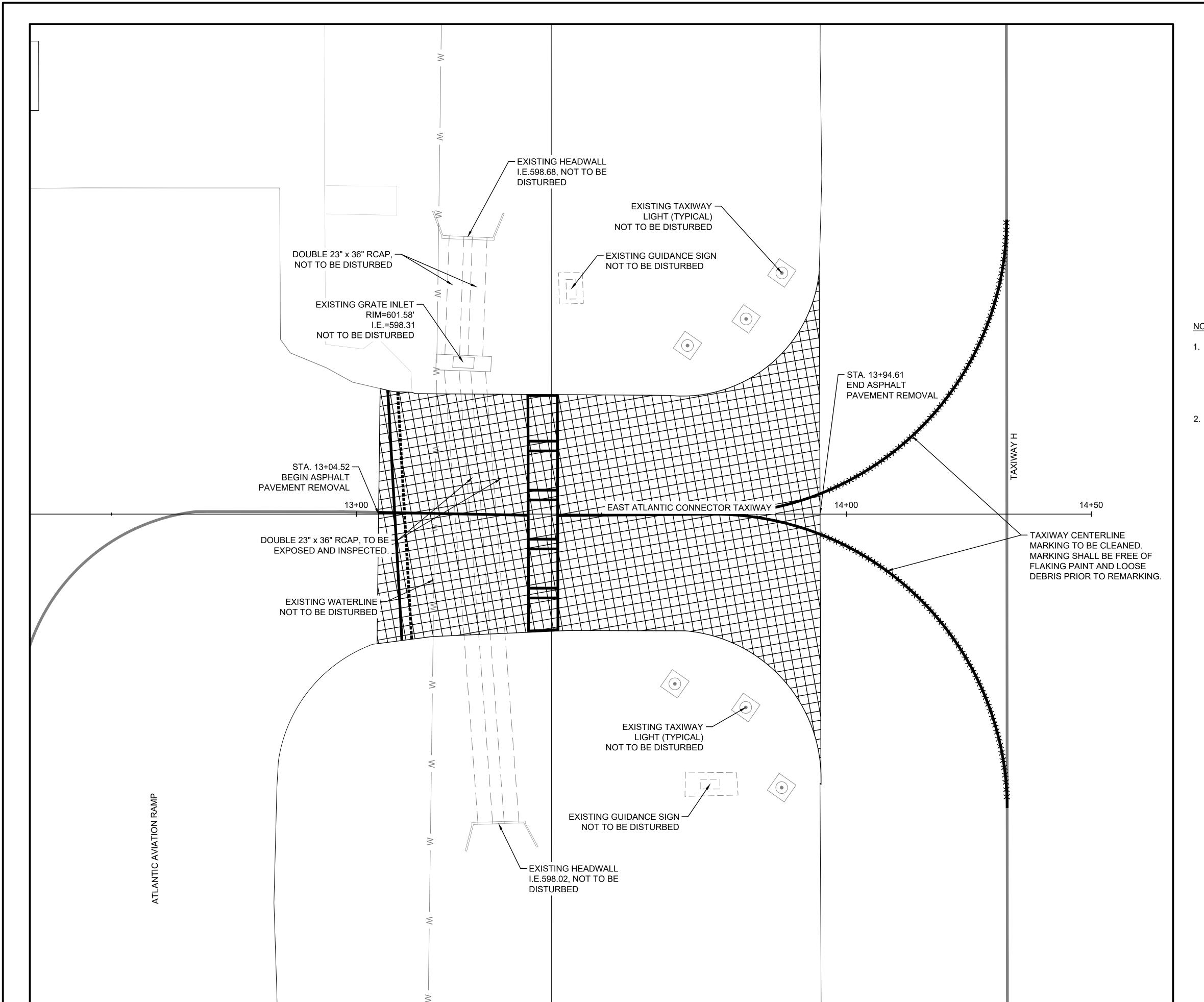
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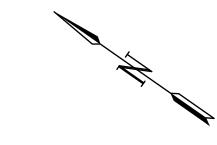
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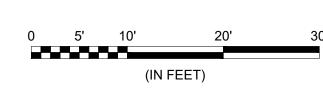
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# **LEGEND** 3" ASPHALT PAVEMENT REMOVAL = = = EXISTING STORM DRAIN PIPE EXISTING DRAINAGE STRUCTURE ------ EXISTING TAXIWAY LIGHT PAVEMENT MARKING TO BE XXXXXXXX

### NOTES:

- 1. CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES, HAND HOLES, AIRFIELD EQUIPMENT, DRAINAGE STRUCTURES, AND BUILDINGS NOT SHOWN FOR DEMOLITION. ANY DAMAGE AS A RESULT OF CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE OWNER.
- 2. WHERE INDICATED IN THE PLANS, THE CONTRACTOR SHALL SAWCUT AT THE PROPOSED EDGE OF CONSTRUCTION, AND 6" INSIDE THE PROPOSED EDGE OF CONSTRUCTION TO PROVIDE A CLEAN VERTICAL EDGE OF EXISTING PAVEMENT. THE CONTRACTOR SHALL NOT REMOVE THE REMAINING 6" PORTION OF PAVEMENT UNTIL NECESSARY FOR CONSTRUCTION OF THE PROPOSED PAVEMENT SECTION.

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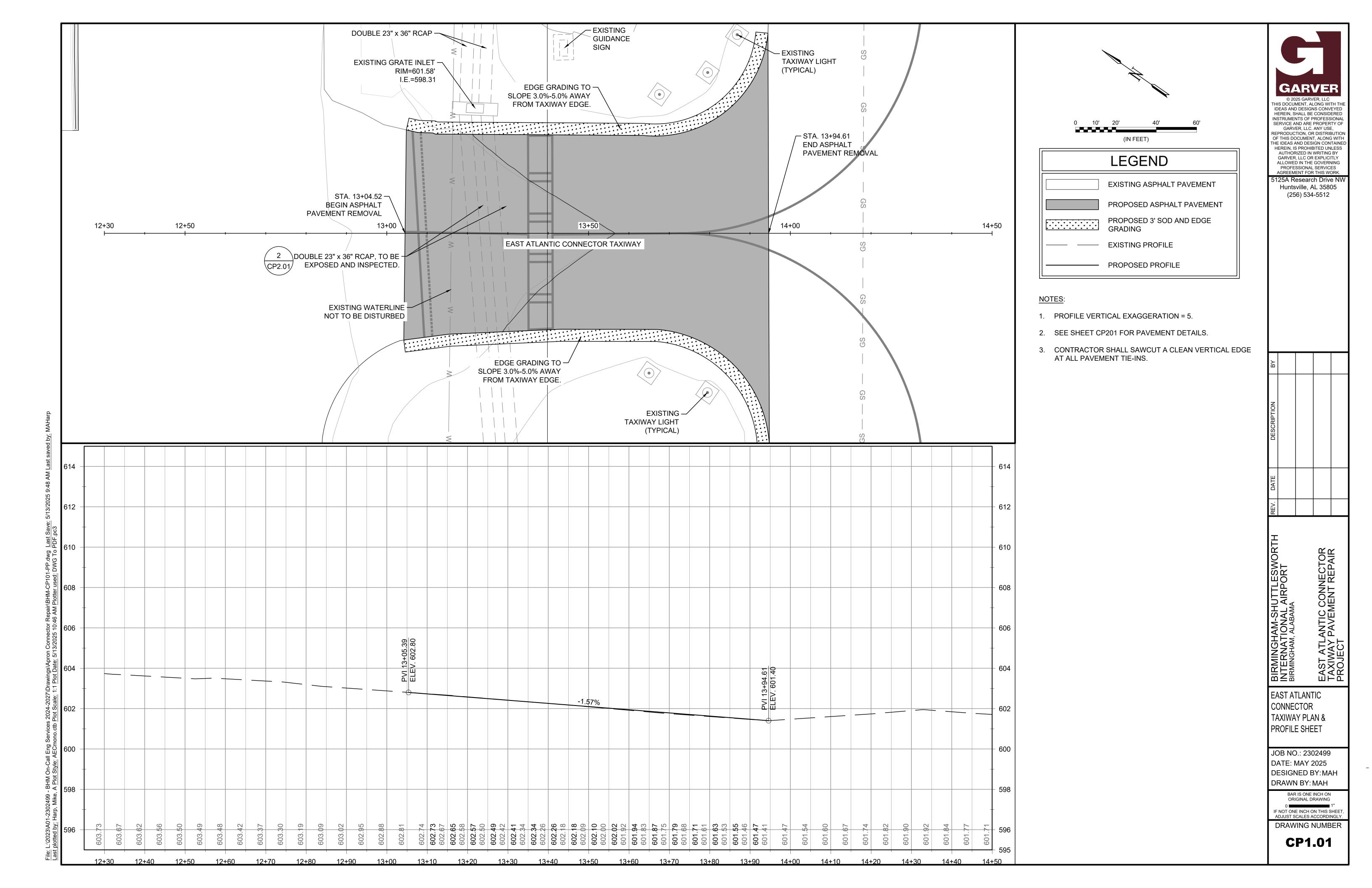
EXISTING **CONDITIONS &** DEMOLITION PLAN

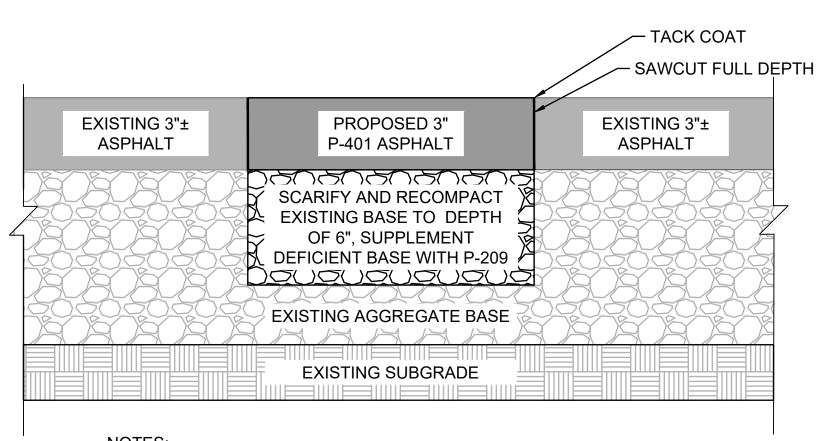
JOB NO.: 2302499 DATE: MAY 2025 DESIGNED BY: MAH DRAWN BY: MAH

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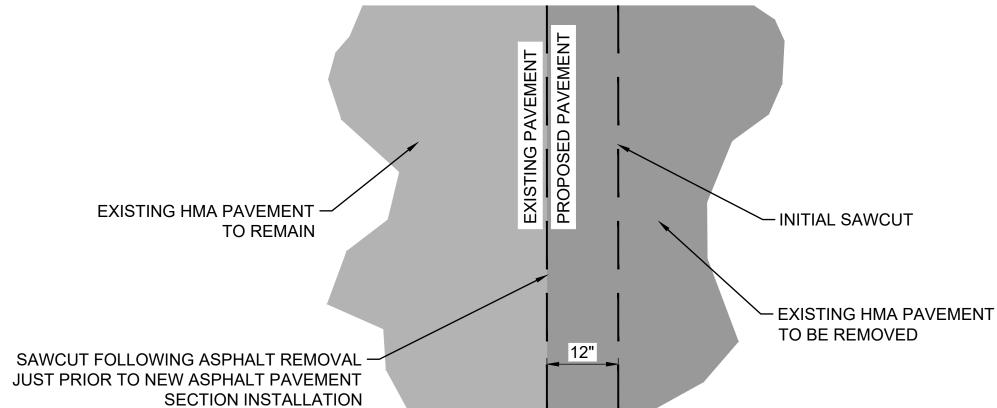
NOTES:

1. CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EXISTING PAVEMENT AND AGGREGATE BASE TO REMAIN. ANY DAMAGE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

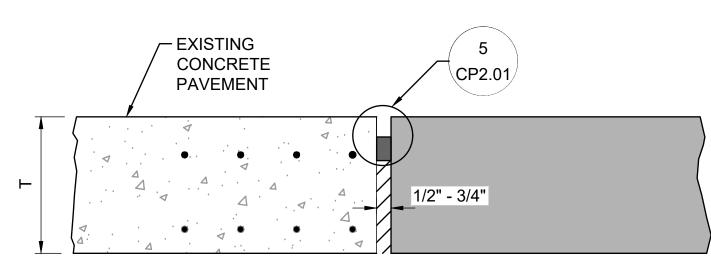


# ASPHALT PAVEMENT FULL DEPTH REPAIR

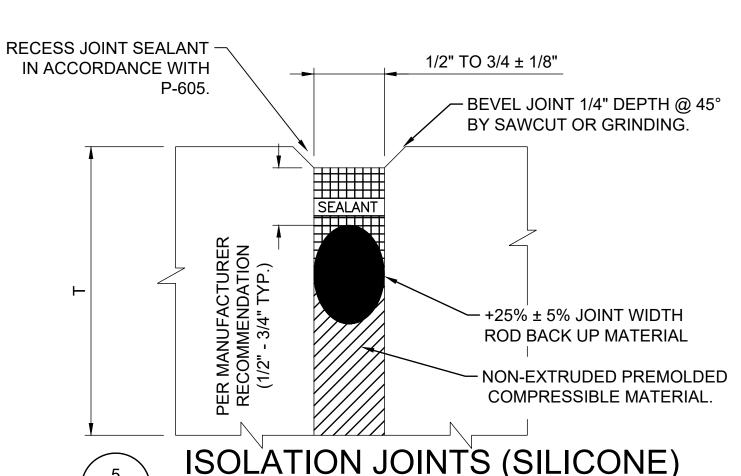
SCALE: NONE



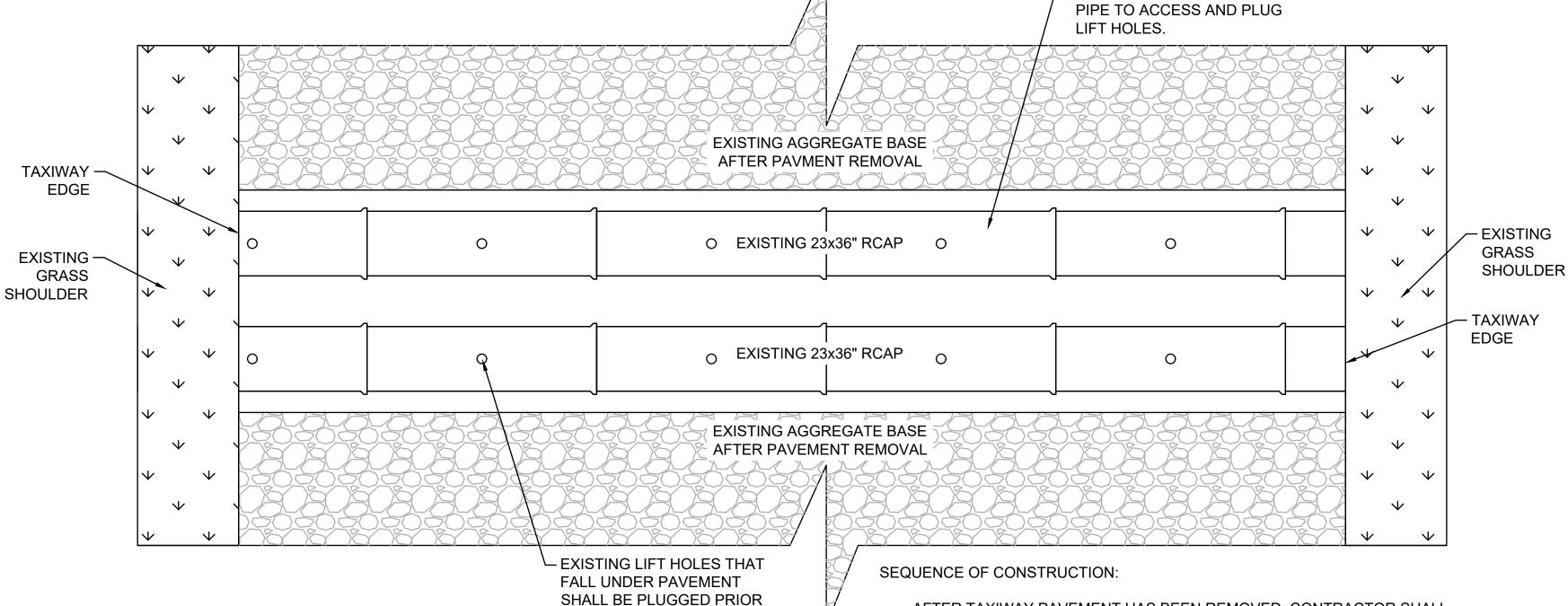




**ISOLATION JOINT AT EXISTING CONCRETE PAVEMENT** CP2.01/ SCALE: NONE



ISOLATION JOINT'S (SILICONE) SCALE: NONE



 AFTER TAXIWAY PAVEMENT HAS BEEN REMOVED, CONTRACTOR SHALL EXPOSE THE TOP OF THE DOUBLE 23"X36" RCAPs THAT WILL REMAIN UNDER PAVEMENT.

CONTRACTOR SHALL

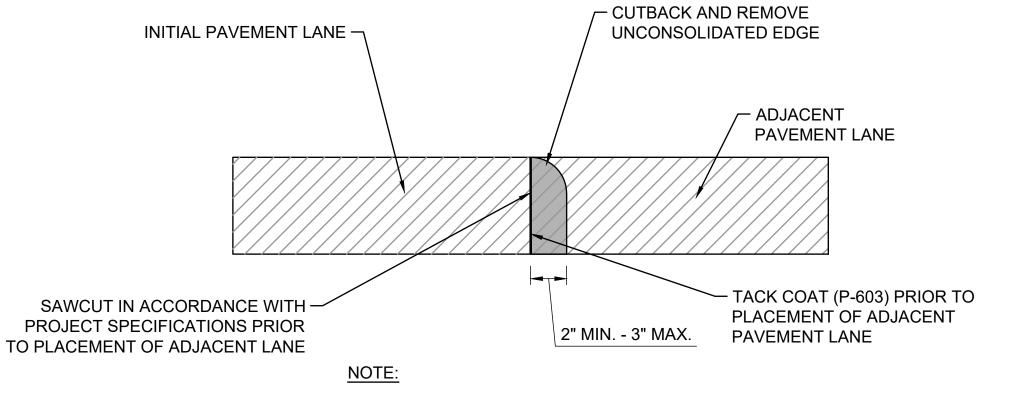
**EXPOSE EXISTING DRAINAGE** 

- CONTRACTOR SHALL THEN PLUG EXISTING LIFT HOLES IN EXPOSED RCAPs BY INSTALLING POPITS BY POPIT, INC. OR APPROVED EQUAL TO MANUFACTURER'S SPECIFICATIONS.
- CONTRACTOR SHALL THEN SPREAD AND RECOMPACT EXISTING BASE AS PER PROJECT SPECIFICATION P-209.

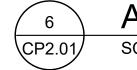
# EXISTING PIPE LIFT HOLE REPAIR DETAIL SCALE: NONE

TO BACKFILLING AND

PAVING. (TYPICAL)



- 1. SAWCUTTING, REMOVAL/DISPOSAL OF CUTBACK MATERIAL, SURFACE PREP, AND TACK COAT SHALL BE INCIDENTAL TO PAVING BID ITEMS AND WILL NOT BE PAID FOR SEPARATELY
- 2. CONTRACTOR SHALL COMPLETE CONTROL STRIP TO DEMONSTRATE CUTBACK PROCEDURE PRODUCES ACCEPTABLE JOINT DENSITIES PRIOR TO PRODUCTION PAVING.
- 3. LONGITUDINAL JOINTS SHALL BE PLACED AND CUTBACK SUCH THAT A MINIMUM OF 1' OFFSET IS MAINTAINED BETWEEN CONSECUTIVE COURSES.



ASPHALT LONGITUDINAL CONSTRUCTION JOINT DETAIL

SCALE: NONE

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BIRMINGHAM-SHUTTLESWORT INTERNATIONAL AIRPORT EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

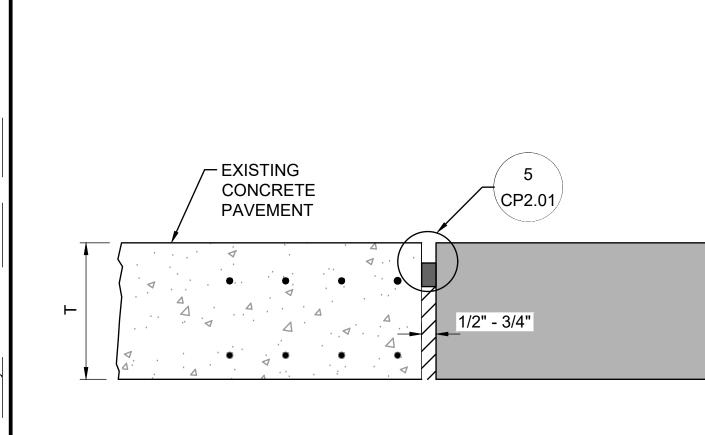
ASPHALT PAVING DETAILS

JOB NO.: 2302499 **DATE: MAY 2025 DESIGNED BY: MAH** DRAWN BY: MAH

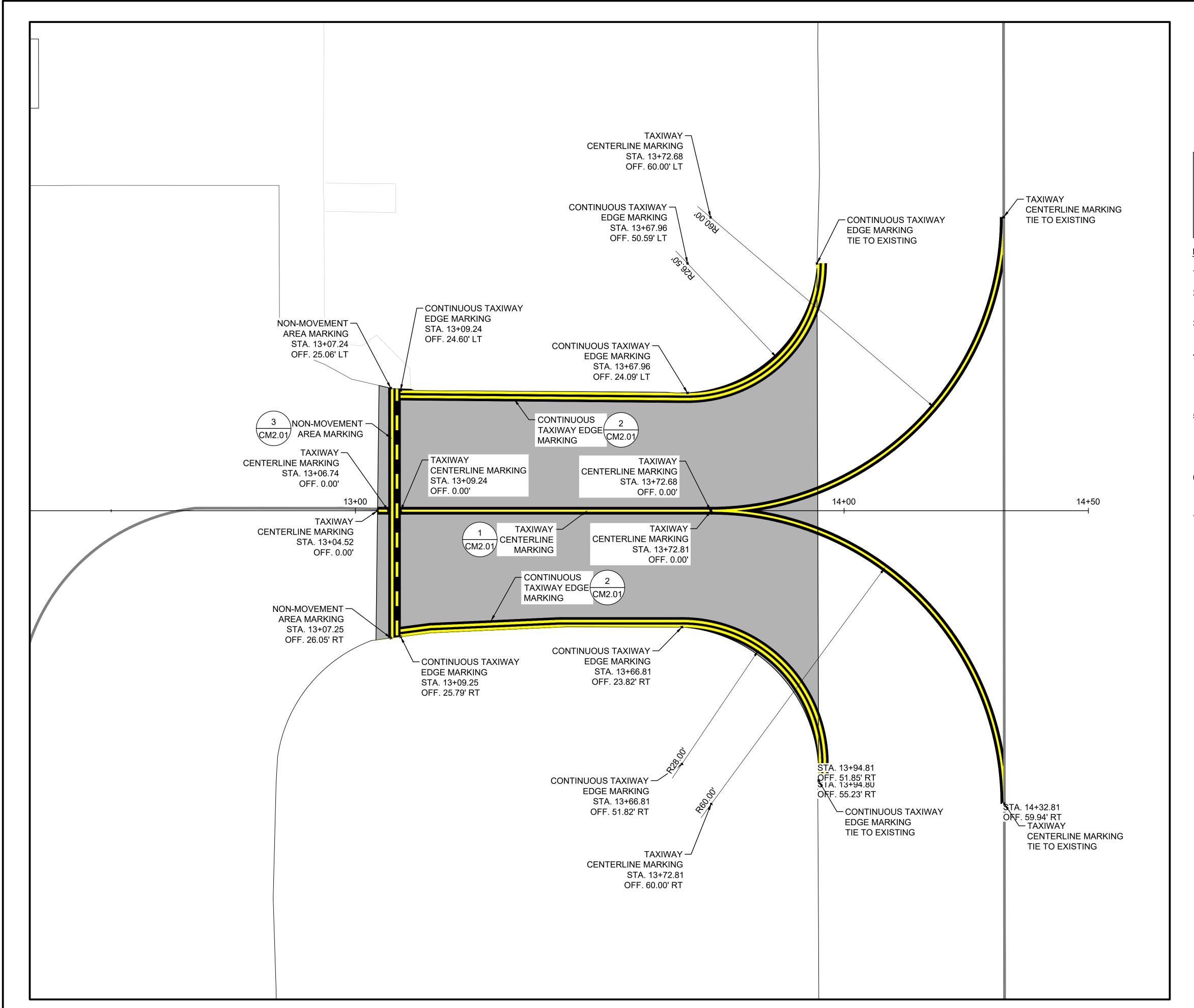
BAR IS ONE INCH ON ORIGINAL DRAWING

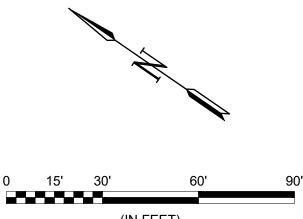
F NOT ONE INCH ON THIS SHEET ADJUST SCALES ACCORDINGLY DRAWING NUMBER

**CP2.01** 



CP2.01





# LEGEND EXISTING MARKINGS PROPOSED YELLOW MARKINGS

# NOTES:

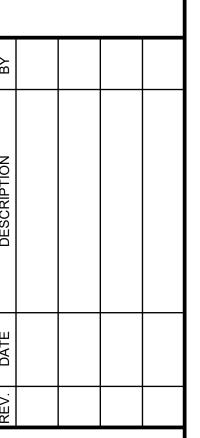
- SHEET INTENDED TO BE PRINTED IN COLOR.
- 2. SEE SHEET CM2.01 FOR AIRFIELD PAVEMENT MARKING DETAILS.
- MARKINGS ON EXISTING PAVEMENT SHOWN FOR REFERENCE ONLY.
- 4. THE CONTRACTOR WILL BE REQUIRED TO REPAINT ANY MARKINGS THAT ARE OUTSIDE OF THE PROJECT LIMITS WHICH ARE DAMAGED BY THE CONTRACTOR'S OPERATIONS. REPAINTING OF DAMAGED AREAS WILL BE AT THE CONTRACTOR'S EXPENSE.
- 5. ANY DISCREPANCIES BETWEEN THESE PLANS AND THE ACTUAL STRIPING PRESENT IN THE FIELD SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO MARKING REMOVAL OR INSTALLATION OPERATIONS.
- 6. MARKINGS TO BE REPAINTED SHALL BE CLEANED AND HAVE ANY LOOSE MATERIAL REMOVED PRIOR TO THE NEW APPLICATION.
- 7. ALL PAVEMENT MARKINGS SHALL BE OUTLINED IN 6" BLACK PAINT UNLESS OTHERWISE NOTED. SEE PAVEMENT MARKING DETAILS SHEETS FOR MORE DETAILS. BLACK PAINT SHALL NOT RECEIVE REFLECTIVE MEDIA. TEMPORARY MARKINGS SHALL NOT RECEIVE BLACK OUTLINE.

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BIRMINGHAM-SHUTTLESWORT
INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR
TAXIWAY PAVEMENT REPAIR
PROJECT

AIRFIELD PAVEMENT MARKING PLAN

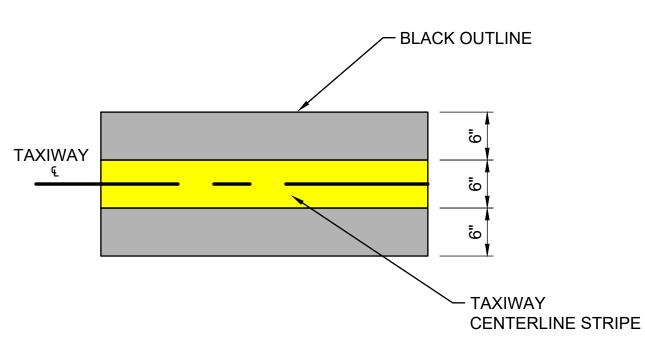
JOB NO.: 2302499 DATE: MAY 2025 DESIGNED BY: MAH DRAWN BY: MAH

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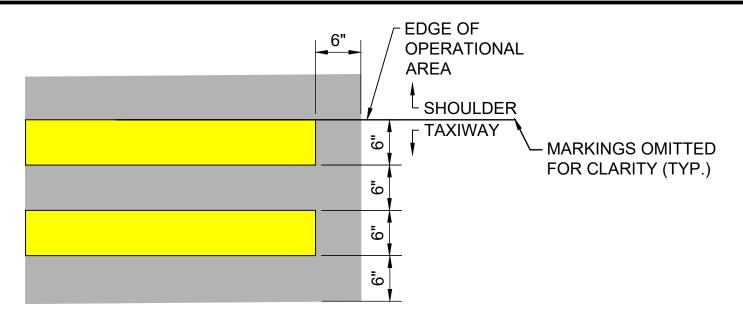
# NOTE:

- 1. ALL YELLOW MARKINGS SHALL HAVE GLASS BEADS.
- 2. ALL BLACK MARKINGS SHALL NOT HAVE GLASS BEADS.
- 3. BLACK OUTLINE SHALL BE 6" OUTSIDE OF YELLOW MARKING.
- 4. DIMENSIONS DO NOT INCLUDE BLACK OUTLINE.



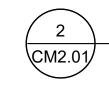
# TAXIWAY CENTERLINE MARKINGS

SCALE: NONE



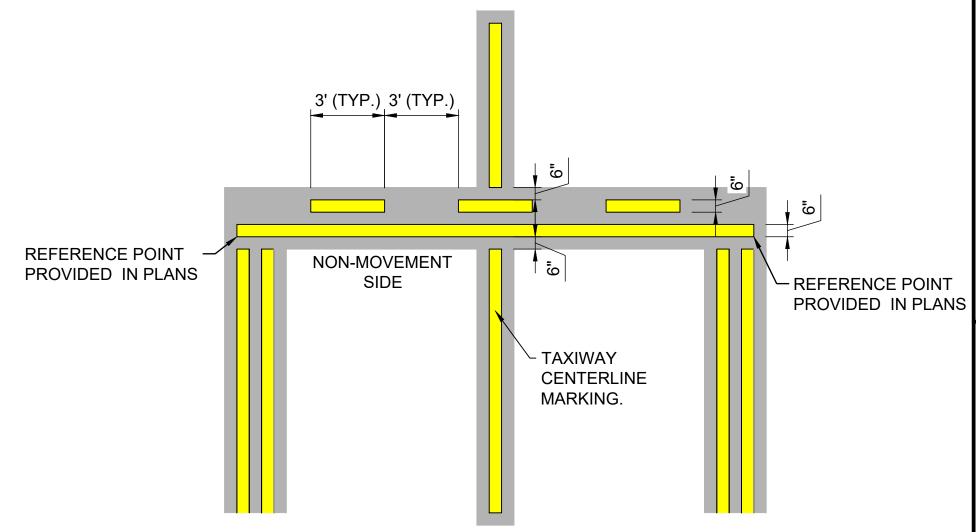
# NOTE:

- 1. ALL YELLOW MARKINGS SHALL HAVE GLASS BEADS.
- 2. ALL BLACK MARKINGS SHALL HAVE NO GLASS BEADS.
- 3. BLACK OUTLINE SHALL BE 6" OUTSIDE OF YELLOW MARKING.
- 4. DIMENSIONS DO NOT INCLUDE BLACK OUTLINE.



# TAXIWAY CONTINUOUS EDGE MARKINGS

SCALE: NONE



NON-MOVEMENT AREA MARKING CM2.01 SCALE: NONE



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AIRFIELD PAVEMENT MARKING DETAILS

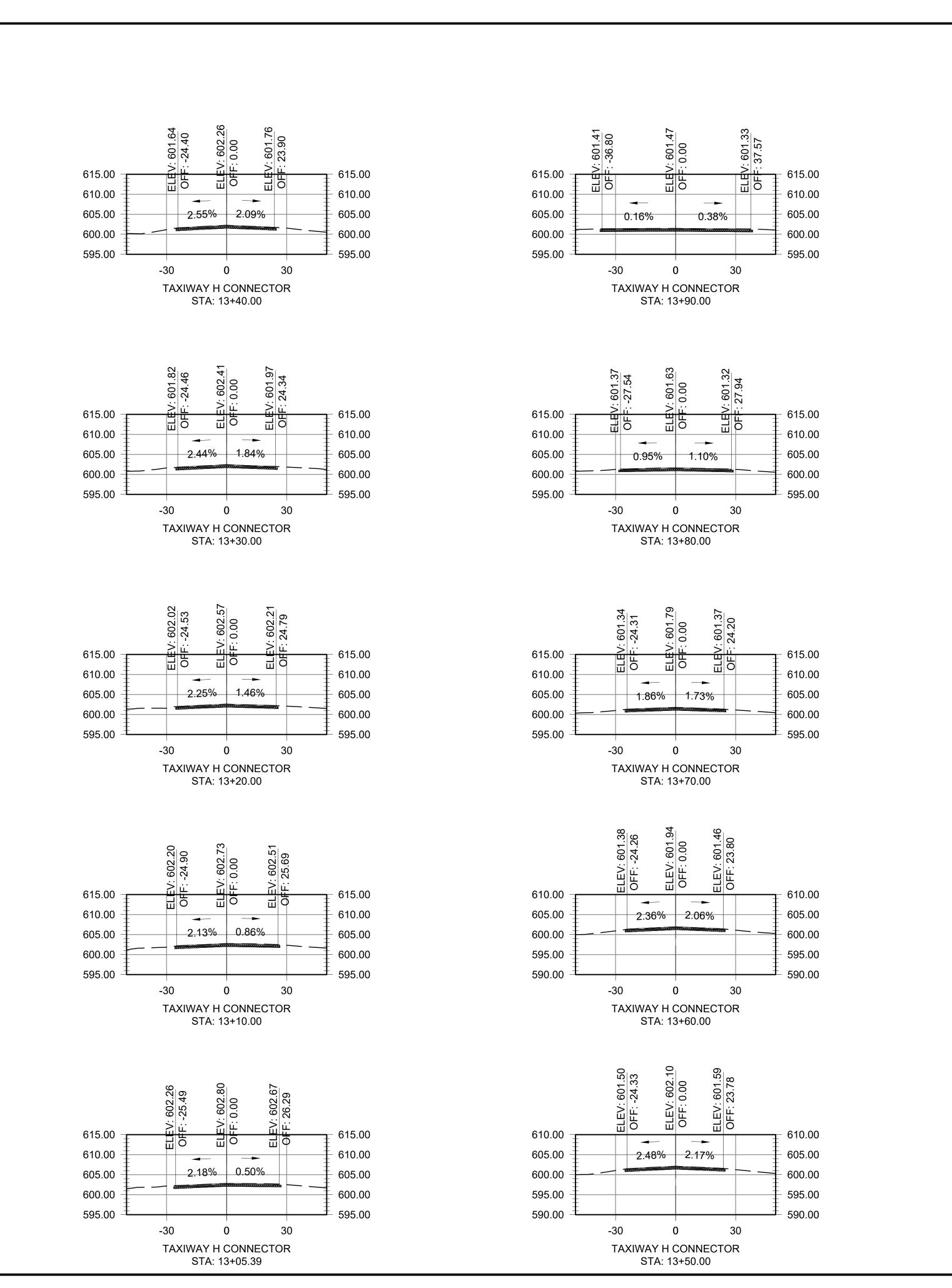
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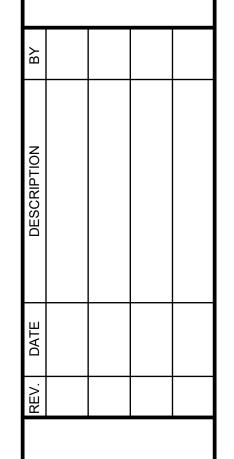


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INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR
TAXIWAY PAVEMENT REPAIR
PROJECT

EAST ATLANTIC CONNECTOR TAXIWAY CROSS SECTIONS

601.34 49.15

0.13%

30

615.00

610.00

605.00

600.00

595.00

0.03%

-30

TAXIWAY H CONNECTOR STA: 13+94.44 615.00

605.00

600.00

595.00

JOB NO.: 2302499 DATE: MAY 2025 DESIGNED BY: MAH DRAWN BY: MAH

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