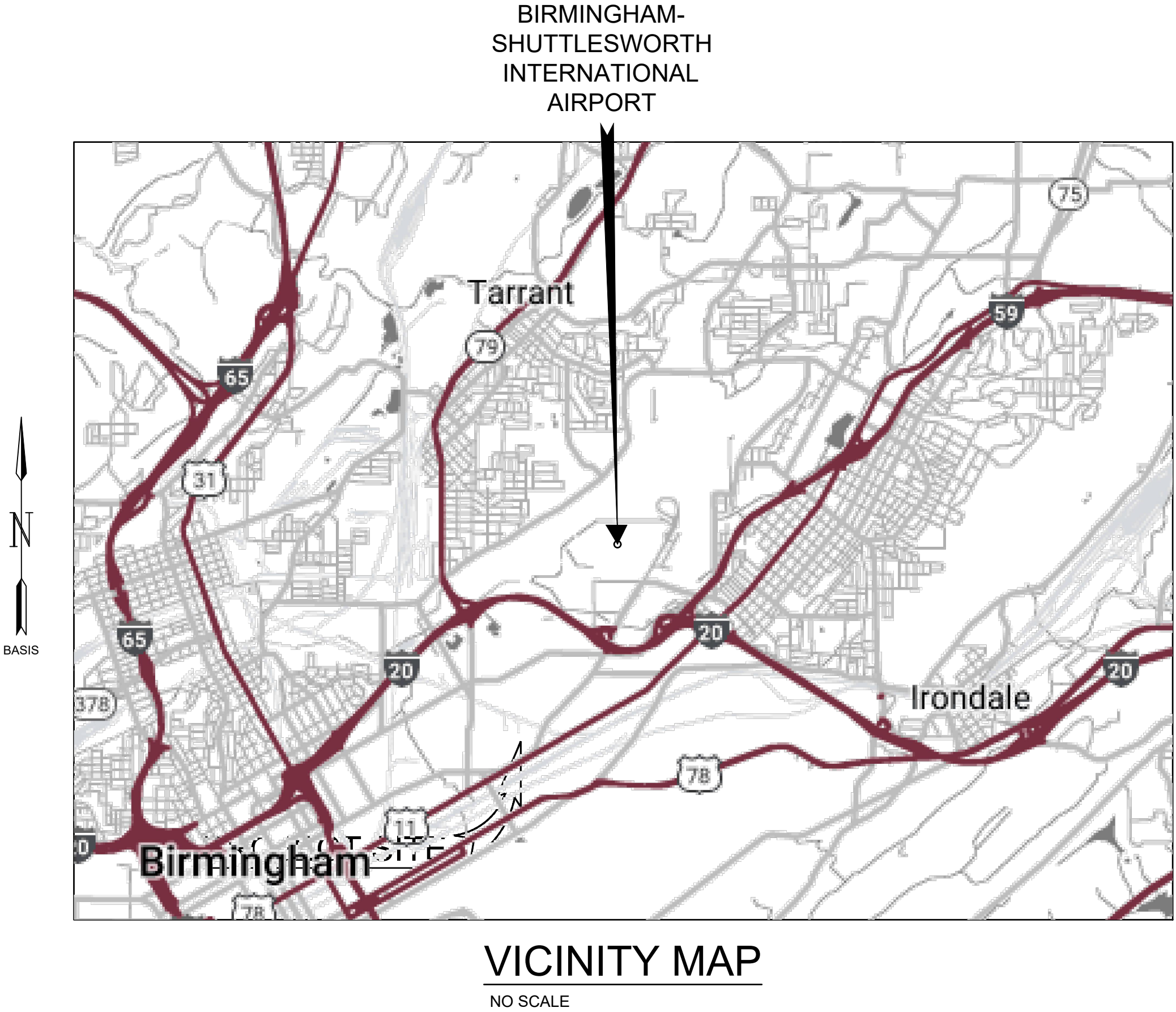
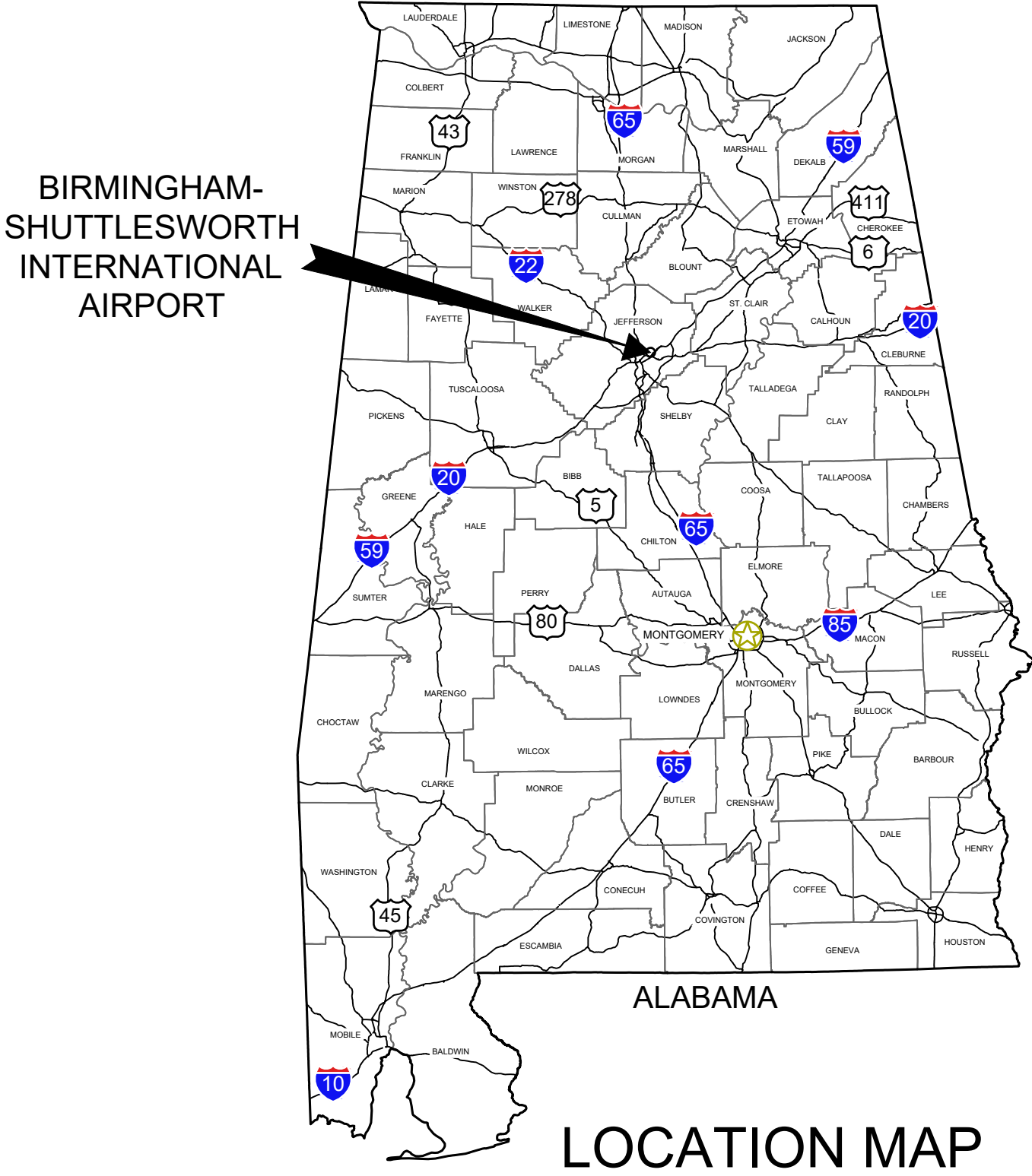



EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT BIRMINGHAM, ALABAMA



SHEET INDEX	
DRAWING NO.	TITLE
GI0.01	COVER SHEET
GI1.01	SUMMARY OF QUANTITIES, PROJECT LAYOUT & SURVEY CONTROL PLAN
GC0.01-GC0.02	CONSTRUCTION SAFETY & PHASING PLAN NOTES
GC1.01	CONSTRUCTION SAFETY & PHASING PLAN
CD1.01	EXISTING CONDITIONS & DEMOLITION PLAN
CP1.01	EAST ATLANTIC CONNECTOR TAXIWAY PLAN & PROFILE SHEET
CP2.01	ASPHALT PAVING DETAILS
CM1.01	AIRFIELD PAVEMENT MARKING PLAN
CM2.01	AIRFIELD PAVEMENT MARKING DETAILS
XS1.01	EAST ATLANTIC CONNECTOR TAXIWAY CROSS SECTIONS

GARVER PROJECT NO. 2302499
MAY 2025





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BIRMINGHAM-SHUTTLESWORTH
INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR
TAXIWAY PAVEMENT REPAIR
PROJECT

COVER SHEET

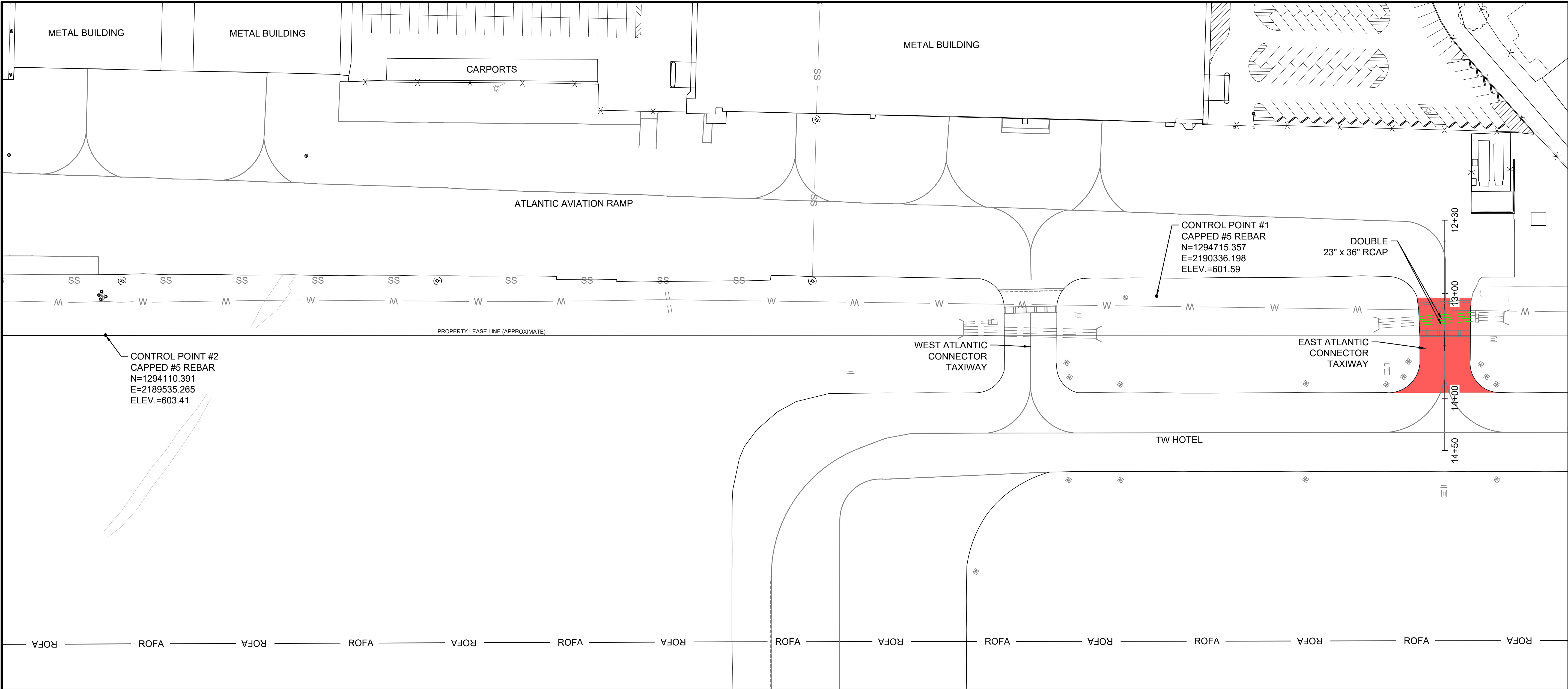
JOB NO.: 2302499
DATE: MAY 2025
DESIGNED BY: MAH
DRAWN BY: MAH

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Last plotted by: Harp, Mike, A Plot Style: AECmono.ctb Plot Date: 5/13/2025 12:31 PM Plotter used: DWG To PDF.pc3



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REV.	DATE	DESCRIPTION	BY

SUMMARY OF QUANTITIES				
ITEM NO.	SPEC. NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY
1	SS-120-3.1	CONSTRUCTION SAFETY AND SECURITY	LS	1
2	SS-150-5.1	RCP LIFT HOLE REPAIR	LS	1
3	SS-220-5.1	PAVEMENT EDGE GRADING	LF	213
4	C-100-14.1	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1
5	C-105-6.1	MOBILIZATION (MAXIMUM 10% OF TOTAL BID)	LS	1
6	P-101-5.1	PAVEMENT REMOVAL	SY	526
7	P-209-5.1	CRUSHED AGGREGATE BASE COURSE, 6-INCH THICKNESS	SY	550
8	P-401-8.1	ASPHALT SURFACE COURSE, 3-INCH THICKNESS	TON	97
9	P-602-5.1	EMULSIFIED ASPHALT PRIME COAT	GAL	165
10	P-620-5.1	PERMANENT STAIN AND ALGAE RESISTANT PAVEMENT MARKING WITH REFLECTIVE MEDIA, YELLOW	SF	404
11	P-620-5.2	PERMANENT STAIN AND ALGAE RESISTANT PAVEMENT MARKING, BLACK	SF	644
12	T-904-5.1	SODDING	SY	70

EAST ATLANTIC CONNECTOR TAXIWAY ALIGNMENT DATA			
DESCRIPTION	STATION	NORTHING	EASTING
POB	12+30.00	1294932.29	2190520.16
POE	14+50.00	1294751.94	2190646.15

ITEMS OF WORK

- CONTRACTOR SHALL REMOVE PAVEMENT IN AREA SPECIFIED IN THE PLANS.
- CONTRACTOR SHALL EXPOSE DOUBLE 23" x 36" ARCH RCP FALLING WITHIN THE PAVEMENT FOOTPRINT AND PLUG ALL OPEN PICK EYE HOLES.
- CONTRACTOR SHALL RE-DRESS AND COMPACT EXISTING CRUSHED AGGREGATE BASE PER PROJECT SPECIFICATIONS.
- CONTRACTOR SHALL PERFORM 3" OVERLAY PER PROJECT PLANS AND SPECIFICATIONS.
- CONTRACTOR SHALL APPLY AIRFIELD PAVEMENT MARKINGS.PER PROJECT PLANS AND SPECIFICATIONS.

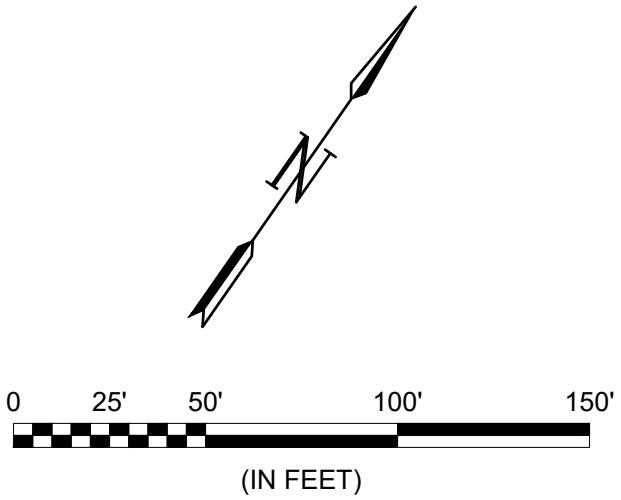
LEGEND

PROPOSED ASPHALT PAVEMENT

EXISTING PIPES TO BE REPAIRED

ROFA RUNWAY OBJECT FREE AREA

- NOTES:
- CONTRACTOR SHALL PROTECT ALL CONTROL POINTS.
 - CONTRACTOR SHALL VERIFY CONTROL POINTS AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO BEGINNING CONSTRUCTION.
 - BASIS FOR BEARINGS SHOWN HEREON IS THE ALABAMA STATE PLANE COORDINATE SYSTEM, NORTH AMERICAN DATUM OF 1983 (NAD 83), ZONE WEST. SURVEY PROVIDED BY AULICK ENGINEERING.



BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

SUMMARY OF QUANTITES, PROJECT LAYOUT & SURVEY CONTROL PLAN

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Last plotted by: Harp, Mike, A Plot Style: AECmono.ctb Plot Date: 5/13/2025 10:45 AM Plotter used: DWG To PDF.pc3

14. RUNWAY AND TAXIWAY VISUAL AIDS
- A. GENERAL - ALL AIRPORT MARKINGS, LIGHTING, SIGNS, AND VISUAL NAVAIDS THAT ARE IN OPERATION MUST BE CLEAR FROM ALL OBSTRUCTIONS. ALL TEMPORARY MARKINGS, SIGNS, LIGHTS, OR OTHER VISUAL AIDS MUST BE SECURED IN PLACE TO PREVENT DAMAGE OR DISPLACEMENT BY PROP WASH, JET BLAST, WING VORTICES, OR OTHER WIND CURRENTS.

B. MARKINGS - ALL TEMPORARY OR PERMANENT RUNWAY AND TAXIWAY VISUAL AIDS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC 150/5340-1 (www.faa.gov). MARKINGS FOR THIS PROJECT INCLUDE THE FOLLOWING:

• TEMPORARILY CLOSED TAXIWAYS - THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, AND MAINTAINING LOW PROFILE BARRICADES AT THE ENTRANCE TO THE CLOSED TAXIWAY FROM AN ADJACENT TAXIWAY. BARRICADES SHALL BE INSTALLED OUTSIDE ALL ACTIVE TAXIWAY SAFETY AREAS. SEE DETAILS ON CONSTRUCTION SAFETY DRAWING FOR LOW-PROFILE AIRCRAFT BARRICADE DETAIL.

C. LIGHTING AND VISUAL NAVAIDS - ALL TEMPORARY LIGHTING FOR RUNWAY AND TAXIWAY SYSTEMS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC 150/5340-30 AND 150/5345-50 (www.faa.gov). THE CONTRACTOR SHALL BE RESPONSIBLE FOR DISCONNECTING ISOLATION TRANSFORMERS ASSOCIATED WITH ANY RUNWAY OR TAXIWAY LIGHT FIXTURES THAT ARE BEING DISCONNECTED.

IF APPLICABLE, ALL CONSTRUCTION, ALTERATION, OR REMOVAL OF FAA OWNED EQUIPMENT SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS SET FORTH IN THE APPROVED FAA REIMBURSABLE AGREEMENT. NO WORK SHALL BE COMPLETED ON FAA OWNED EQUIPMENT PRIOR TO COMPLETION OF THE FAA REIMBURSABLE AGREEMENT.

D. SIGNS - THE CONTRACTOR SHALL INSTALL ALL SIGNS IN ACCORDANCE WITH THE MOST RECENT EDITION OF FAA AC 150/5345-44 AND 150/5340-18. ANY SIGN THAT IS NOT PERFORMING ITS NORMAL FUNCTION MUST BE COVERED OR REMOVED TO PREVENT MISLEADING PILOTS.

15. MARKING AND SIGNS FOR ACCESS ROUTES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING AND INSTALLING ALL NECESSARY MARKINGS AND SIGNAGE FOR ALL ACCESS ROUTES TO AND FROM THE SITE TO BE USED BY CONTRACTOR PERSONNEL, SUBCONTRACTOR PERSONNEL, OR DELIVERY OPERATIONS. ALL SIGNAGE IN THE AIR OPERATIONS AREA SHALL BE FRANGIBLY MOUNTED.

16. HAZARD MARKING AND LIGHTING

A. PURPOSE - HAZARD MARKING AND LIGHTING PREVENTS PILOTS FROM ENTERING AREAS CLOSED TO AIRCRAFT AND PREVENTS CONTRACTOR PERSONNEL FROM ENTERING AREAS OPEN TO AIRCRAFT.

B. EQUIPMENT - THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN LOW-PROFILE BARRICADES IN HAZARDOUS AREAS INSIDE MOVEMENT AREAS. BARRICADES SHALL RESTRICT ACCESS AND MAKE HAZARDS OBVIOUS TO AIRCRAFT, PERSONNEL, AND VEHICLES. DURING PERIODS OF LOW VISIBILITY AND AT NIGHT, BARRICADES SHALL BE EQUIPPED WITH RED FLASHING OR STEADY BURNING LIGHTS. THE SPACING OF BARRICADES SHALL BE SUCH THAT A BREACH IS PHYSICALLY PREVENTED BARRING A DELIBERATE ACT. IF BARRICADES ARE INTENDED TO PREVENT PEDESTRIANS, THEN THEY SHALL BE LINKED. SEE DETAILS ON CONSTRUCTION SAFETY DRAWINGS FOR LOW-PROFILE AIRCRAFT BARRICADE DETAIL.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

ALL WORK CONDUCTED AT NIGHT SHALL BE ACCOMPANIED BY ADEQUATE LIGHT FACILITIES TO COMPLETE THE WORK. ALL LIGHT FACILITIES SHALL BE AIMED OR SHIELDED AS NECESSARY TO AVOID IMPACTING AIRCRAFT OR ATCT OPERATIONS. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A LIGHTING PLAN SHOWING THE LOCATION AND AIMING DIRECTION OF ALL LIGHT FACILITIES PRIOR TO THE COMPLETION OF ANY NIGHT WORK.

18. PROTECTION OF SAFETY AREAS, OBJECT FREE AREAS, OBJECT FREE ZONES, AND APPROACH/DEPARTURE SURFACES.

A. RUNWAY SAFETY AREAS (RSA) - NO WORK SHALL BE PERMITTED WITHIN AN ACTIVE RUNWAY SAFETY AREA.

B. RUNWAY OBJECT FREE AREAS (ROFA) - NO MATERIAL SHALL BE STOCKPILED INSIDE THE LIMITS OF THE ACTIVE ROFA.

C. TAXIWAY SAFETY AREAS (TSA) - NO WORK SHALL BE PERMITTED WITHIN AN ACTIVE TSA.

D. TAXIWAY OBJECT FREE AREAS (TOFA) - NO CONSTRUCTION SHALL BE PERMITTED INSIDE AN ACTIVE TOFA.

E. OBSTACLE FREE ZONE (OFZ) - NO PERSONNEL, MATERIAL, OR EQUIPMENT SHALL PENETRATE THE OFZ WHILE THE RUNWAY IS OPEN TO OPERATIONS. THE DIMENSIONS OF THE OFZ ARE AS DEFINED IN FAA AC 150/5300-13 (www.faa.gov).


F. APPROACH/DEPARTURE SURFACES - ALL CONTRACTOR PERSONNEL, MATERIALS, AND EQUIPMENT SHALL REMAIN CLEAR OF THE APPLICABLE THRESHOLD SITING SURFACES AS DEFINED IN CHAPTER 3 OF FAA AC 150/5300-13 (www.faa.gov).

19. OTHER LIMITATIONS ON CONSTRUCTION

A. PROHIBITIONS - THE USE OF TALL EQUIPMENT (I.E. CRANES, CONCRETE PUMPS) SHALL NOT BE PERMITTED UNLESS APPROVED BY THE ENGINEER.

OPEN FLAME WELDING AND TORCH CUTTING OPERATIONS ARE NOT PERMITTED UNLESS ADEQUATE FIRE SAFETY PRECAUTIONS ARE PROVIDED AND THESE OPERATIONS ARE AUTHORIZED BY THE AIRPORT OPERATOR AND THE ENGINEER.

ELECTRICAL BLASTING CAPS SHALL NOT BE PERMITTED WITHIN 1,000-FT OF THE AIRPORT PROPERTY. FLARE POTS ARE NOT PERMITTED WITHIN THE AIR OPERATIONS AREA.

B. RESTRICTIONS - NONE
- 

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BY				
DESCRIPTION				
DATE				
REV.				

BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

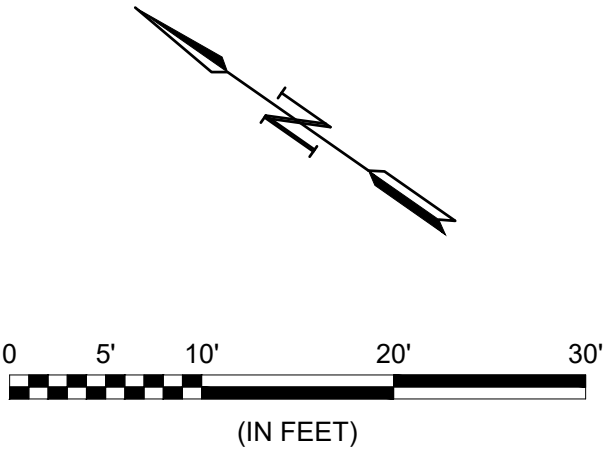
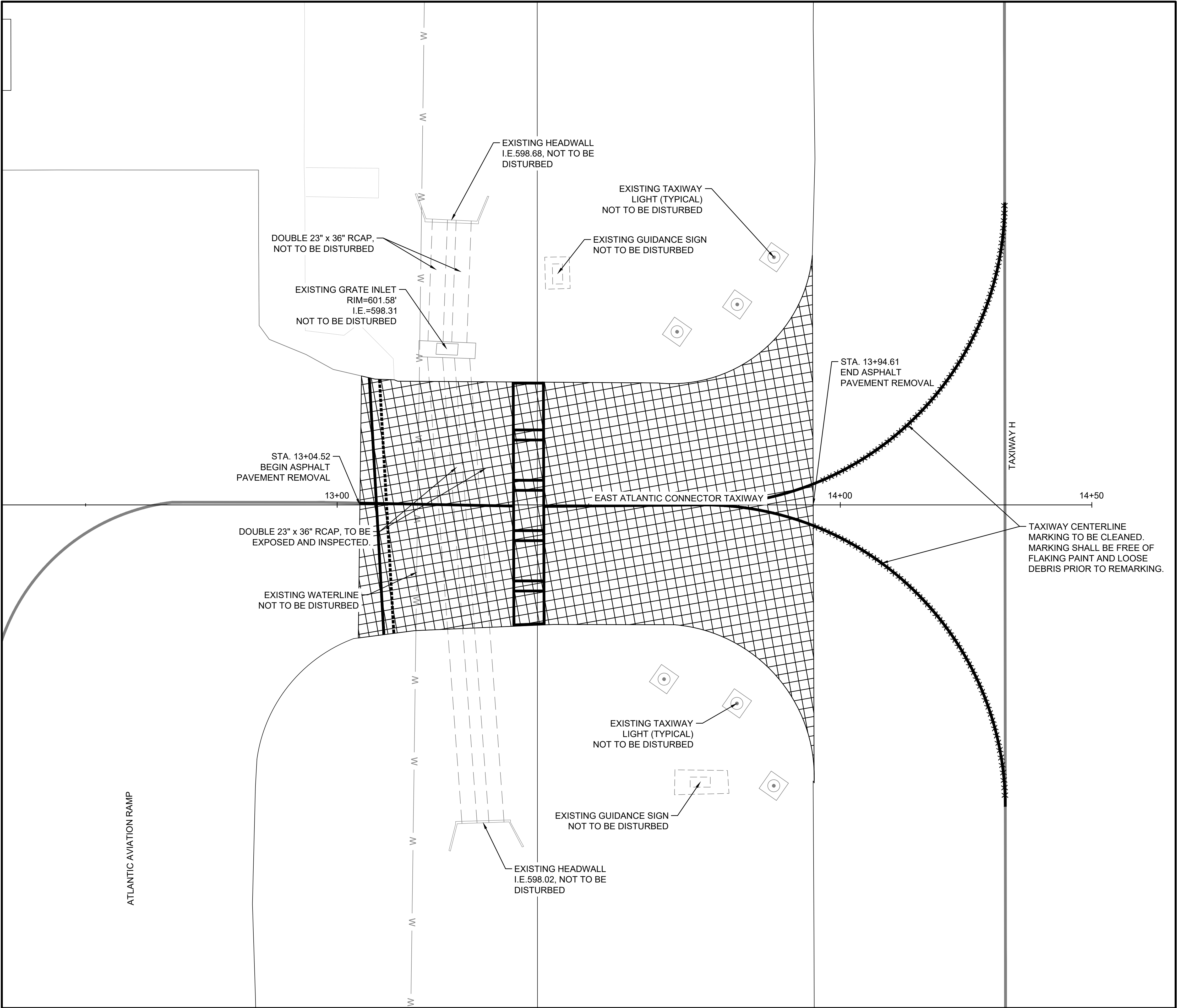
CONSTRUCTION
SAFETY & PHASING
PLAN NOTES

JOB NO.: 2302499
DATE: MAY 2025
DESIGNED BY: MAH
DRAWN BY: MAH

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LEGEND

3" ASPHALT PAVEMENT REMOVAL

EXISTING STORM DRAIN PIPE

EXISTING DRAINAGE STRUCTURE

EXISTING TAXIWAY LIGHT

PAVEMENT MARKING TO BE CLEANED

- NOTES:
- CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES, HAND HOLES, AIRFIELD EQUIPMENT, DRAINAGE STRUCTURES, AND BUILDINGS NOT SHOWN FOR DEMOLITION. ANY DAMAGE AS A RESULT OF CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE OWNER.
 - WHERE INDICATED IN THE PLANS, THE CONTRACTOR SHALL SAWCUT AT THE PROPOSED EDGE OF CONSTRUCTION, AND 6" INSIDE THE PROPOSED EDGE OF CONSTRUCTION TO PROVIDE A CLEAN VERTICAL EDGE OF EXISTING PAVEMENT. THE CONTRACTOR SHALL NOT REMOVE THE REMAINING 6" PORTION OF PAVEMENT UNTIL NECESSARY FOR CONSTRUCTION OF THE PROPOSED PAVEMENT SECTION.

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BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

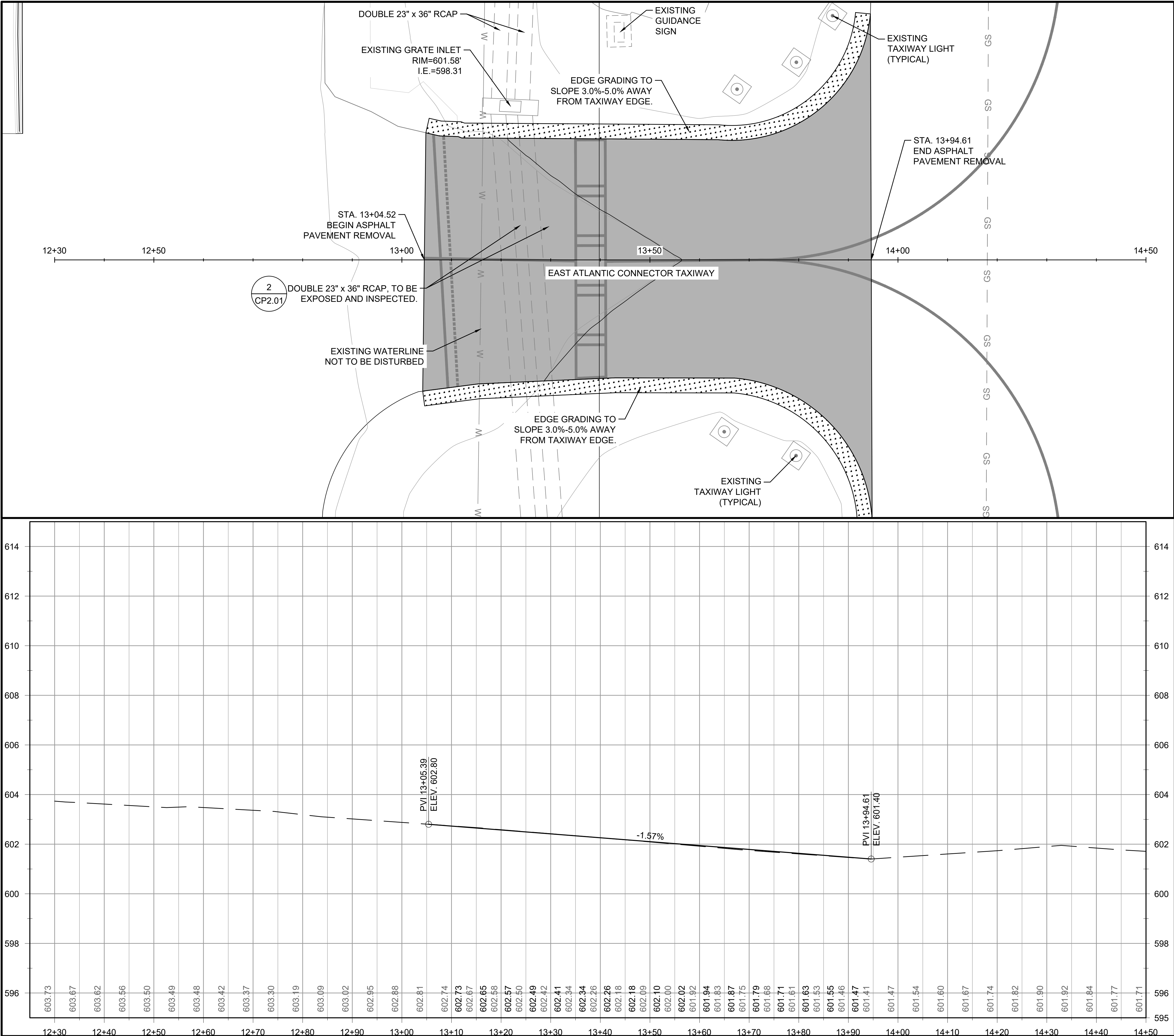
EXISTING CONDITIONS & DEMOLITION PLAN

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LEGEND

	EXISTING ASPHALT PAVEMENT
	PROPOSED ASPHALT PAVEMENT
	PROPOSED 3' SOD AND EDGE GRADING
	EXISTING PROFILE
	PROPOSED PROFILE

- NOTES:
- PROFILE VERTICAL EXAGGERATION = 5.
 - SEE SHEET CP201 FOR PAVEMENT DETAILS.
 - CONTRACTOR SHALL SAWCUT A CLEAN VERTICAL EDGE AT ALL PAVEMENT TIE-INS.

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BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

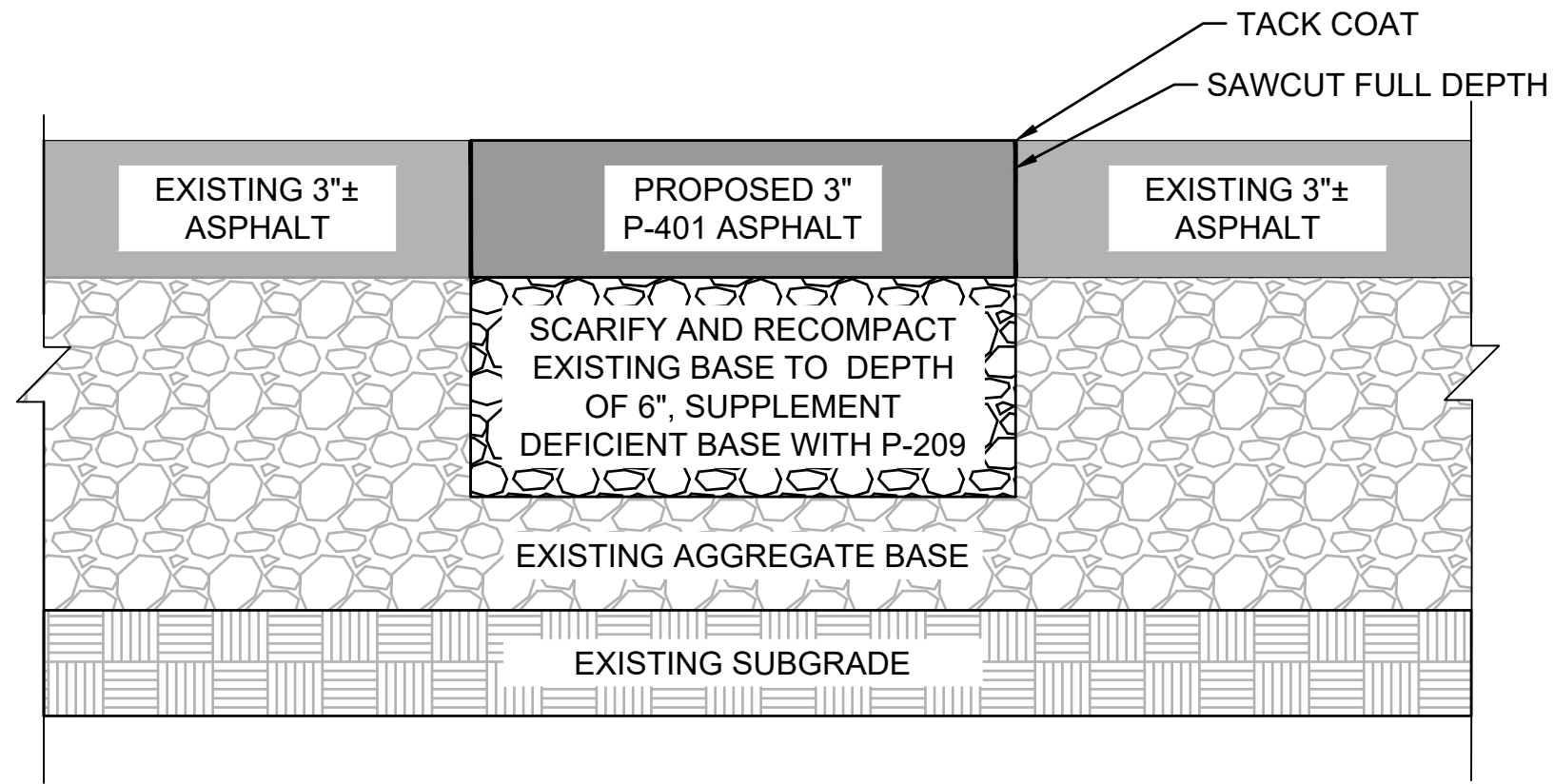
EAST ATLANTIC CONNECTOR TAXIWAY PLAN & PROFILE SHEET

JOB NO.: 2302499
DATE: MAY 2025
DESIGNED BY: MAH
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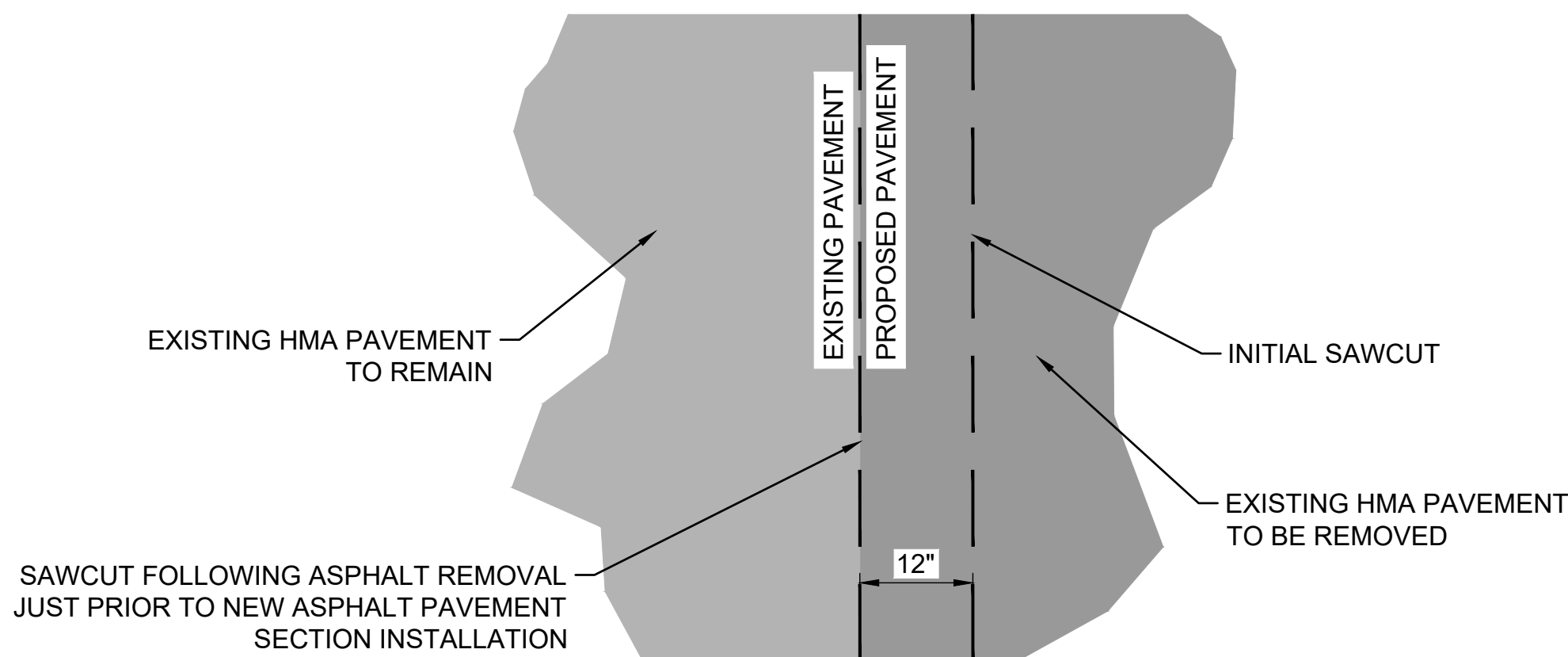
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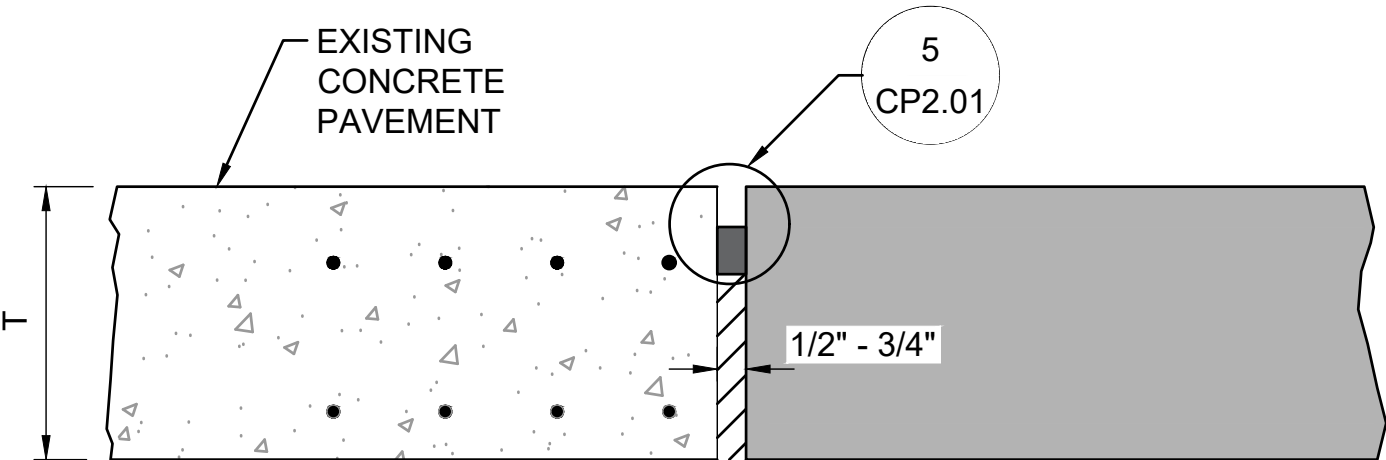
NOTES:

- CONTRACTOR IS RESPONSIBLE FOR PROTECTING THE EXISTING PAVEMENT AND AGGREGATE BASE TO REMAIN. ANY DAMAGE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

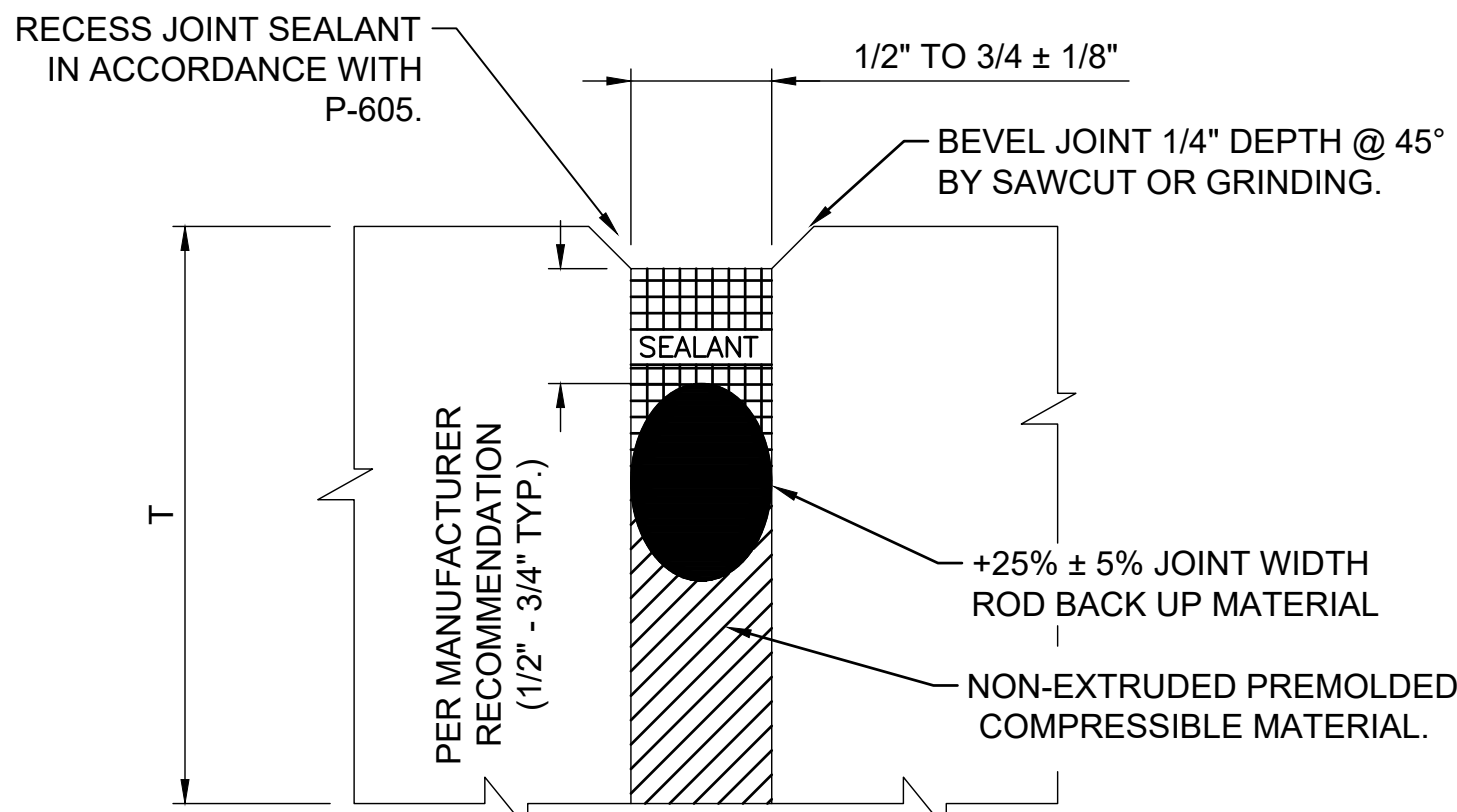
1
CP2.01
ASPHALT PAVEMENT FULL DEPTH REPAIR
SCALE: NONE



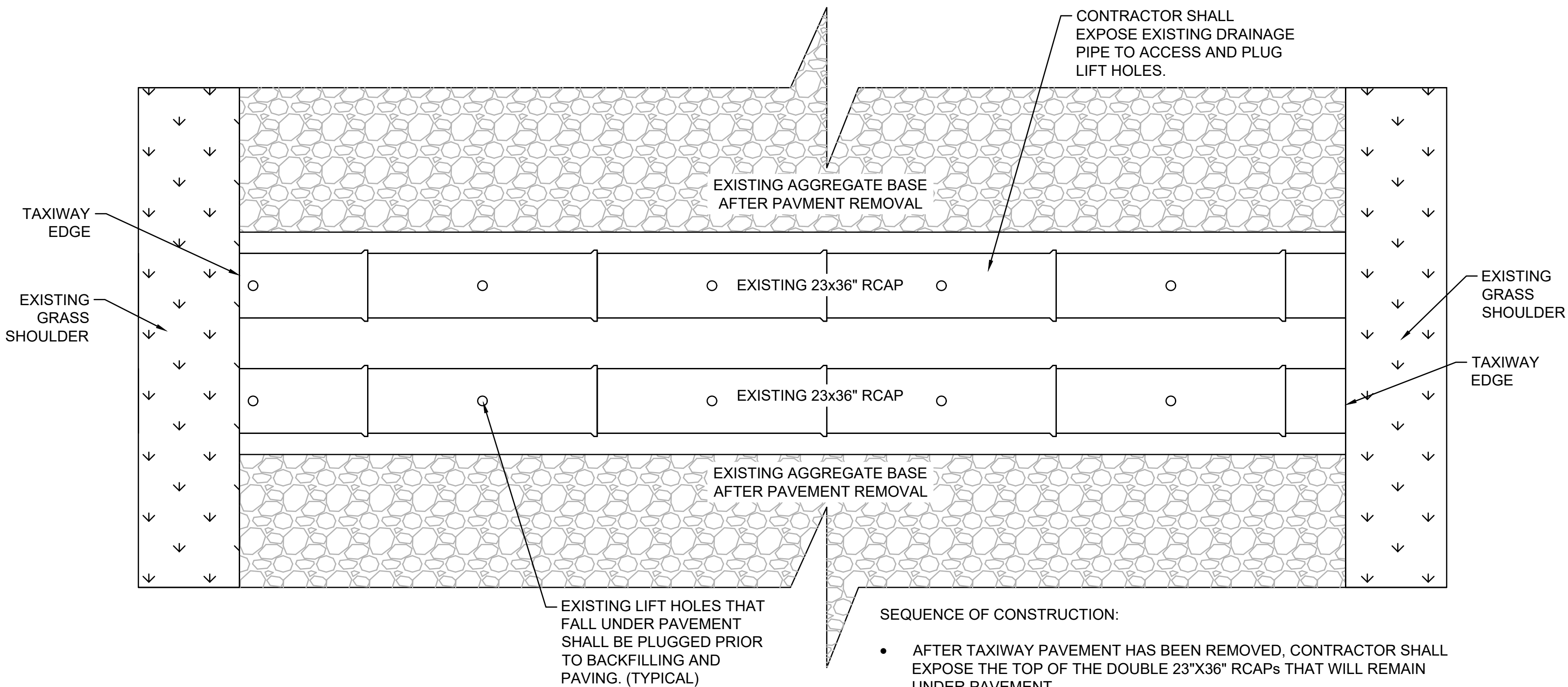
3
CP2.01
HMA DEMOLITION DETAIL
SCALE: NONE



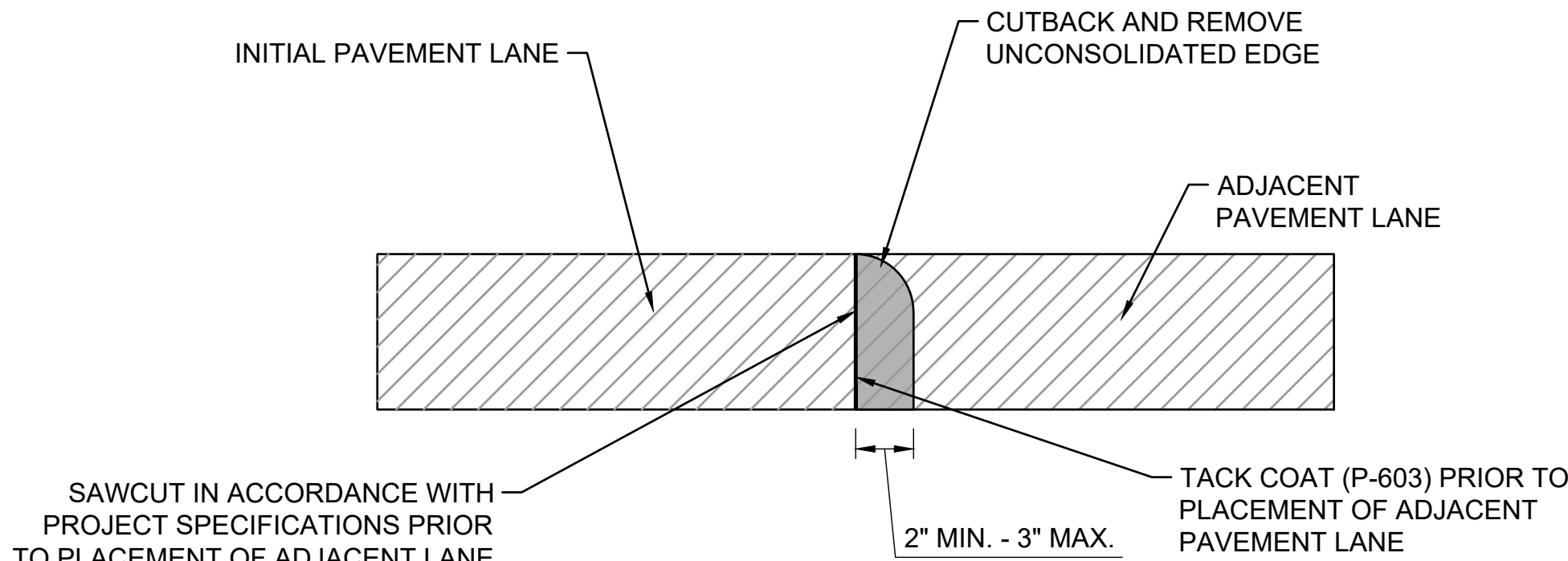
4
CP2.01
ISOLATION JOINT AT EXISTING CONCRETE PAVEMENT
SCALE: NONE



5
CP2.01
ISOLATION JOINTS (SILICONE)
SCALE: NONE




2
CP2.01
EXISTING PIPE LIFT HOLE REPAIR DETAIL
SCALE: NONE



NOTE:

- SAWCUTTING, REMOVAL/DISPOSAL OF CUTBACK MATERIAL, SURFACE PREP, AND TACK COAT SHALL BE INCIDENTAL TO PAVING BID ITEMS AND WILL NOT BE PAID FOR SEPARATELY.
- CONTRACTOR SHALL COMPLETE CONTROL STRIP TO DEMONSTRATE CUTBACK PROCEDURE PRODUCES ACCEPTABLE JOINT DENSITIES PRIOR TO PRODUCTION PAVING.
- LONGITUDINAL JOINTS SHALL BE PLACED AND CUTBACK SUCH THAT A MINIMUM OF 1' OFFSET IS MAINTAINED BETWEEN CONSECUTIVE COURSES.

6
CP2.01
ASPHALT LONGITUDINAL CONSTRUCTION JOINT DETAIL
SCALE: NONE



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BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

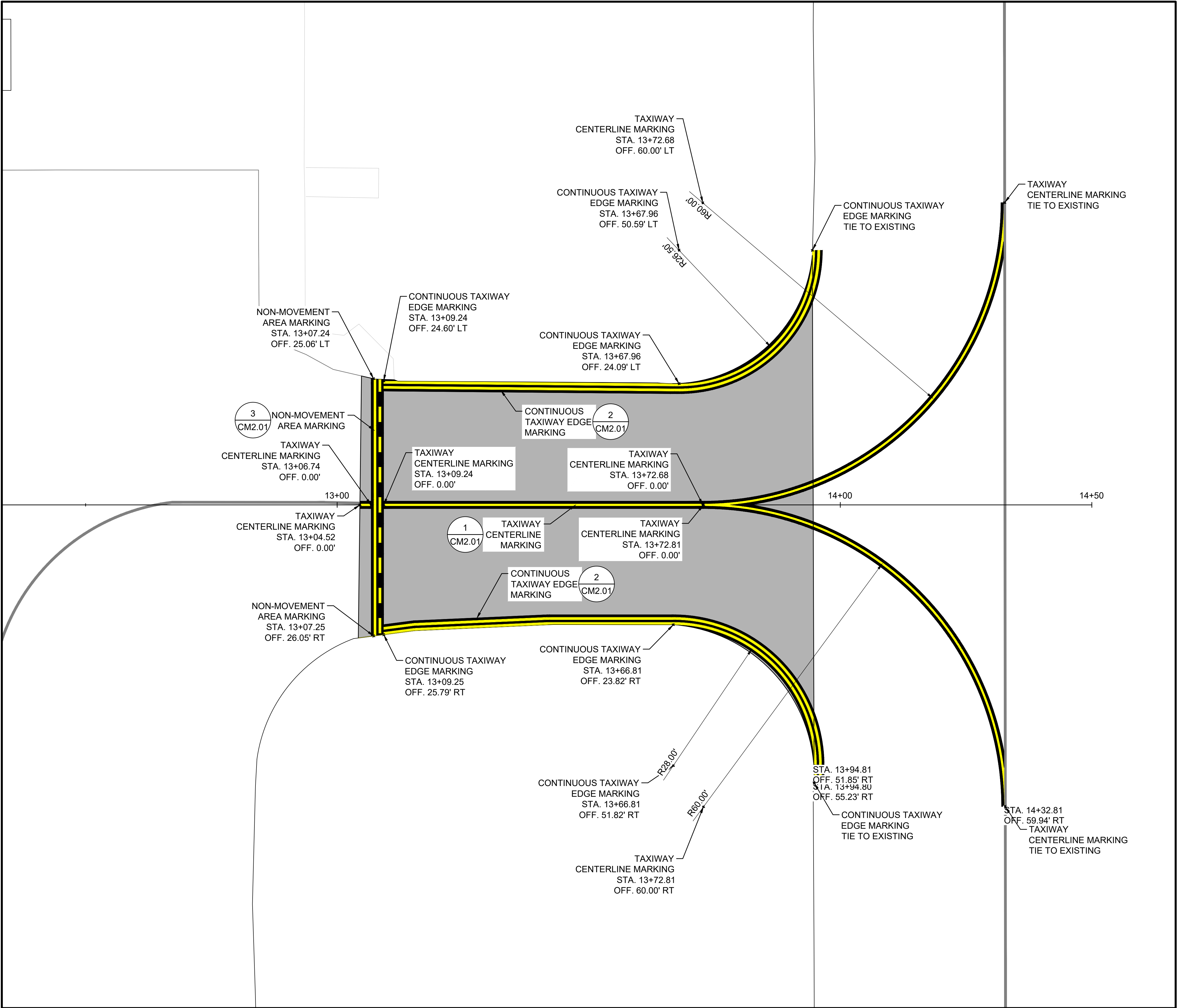
ASPHALT PAVING DETAILS

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(IN FEET)

LEGEND

EXISTING MARKINGS

PROPOSED YELLOW MARKINGS

- NOTES:
- SHEET INTENDED TO BE PRINTED IN COLOR.
 - SEE SHEET CM2.01 FOR AIRFIELD PAVEMENT MARKING DETAILS.
 - MARKINGS ON EXISTING PAVEMENT SHOWN FOR REFERENCE ONLY.
 - THE CONTRACTOR WILL BE REQUIRED TO REPAINT ANY MARKINGS THAT ARE OUTSIDE OF THE PROJECT LIMITS WHICH ARE DAMAGED BY THE CONTRACTOR'S OPERATIONS. REPAINTING OF DAMAGED AREAS WILL BE AT THE CONTRACTOR'S EXPENSE.
 - ANY DISCREPANCIES BETWEEN THESE PLANS AND THE ACTUAL STRIPING PRESENT IN THE FIELD SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO MARKING REMOVAL OR INSTALLATION OPERATIONS.
 - MARKINGS TO BE REPAINTED SHALL BE CLEANED AND HAVE ANY LOOSE MATERIAL REMOVED PRIOR TO THE NEW APPLICATION.
 - ALL PAVEMENT MARKINGS SHALL BE OUTLINED IN 6" BLACK PAINT UNLESS OTHERWISE NOTED. SEE PAVEMENT MARKING DETAILS SHEETS FOR MORE DETAILS. BLACK PAINT SHALL NOT RECEIVE REFLECTIVE MEDIA. TEMPORARY MARKINGS SHALL NOT RECEIVE BLACK OUTLINE.

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BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

AIRFIELD PAVEMENT MARKING PLAN

JOB NO.: 2302499
DATE: MAY 2025
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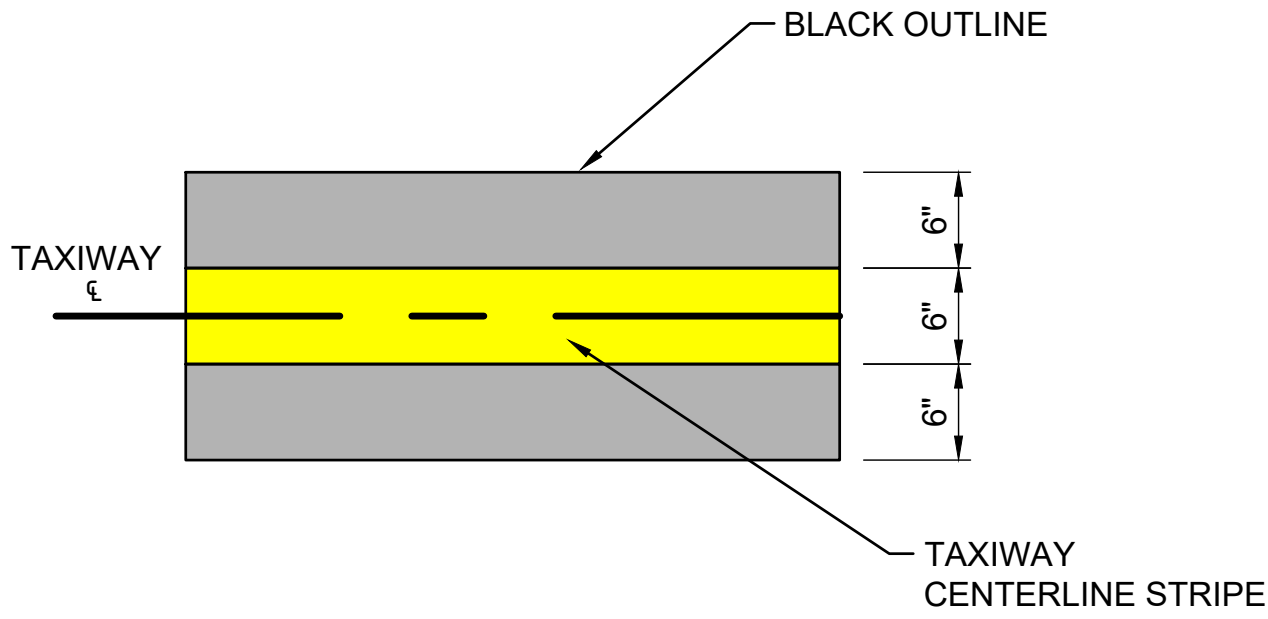
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DRAWING NUMBER

CM1.01

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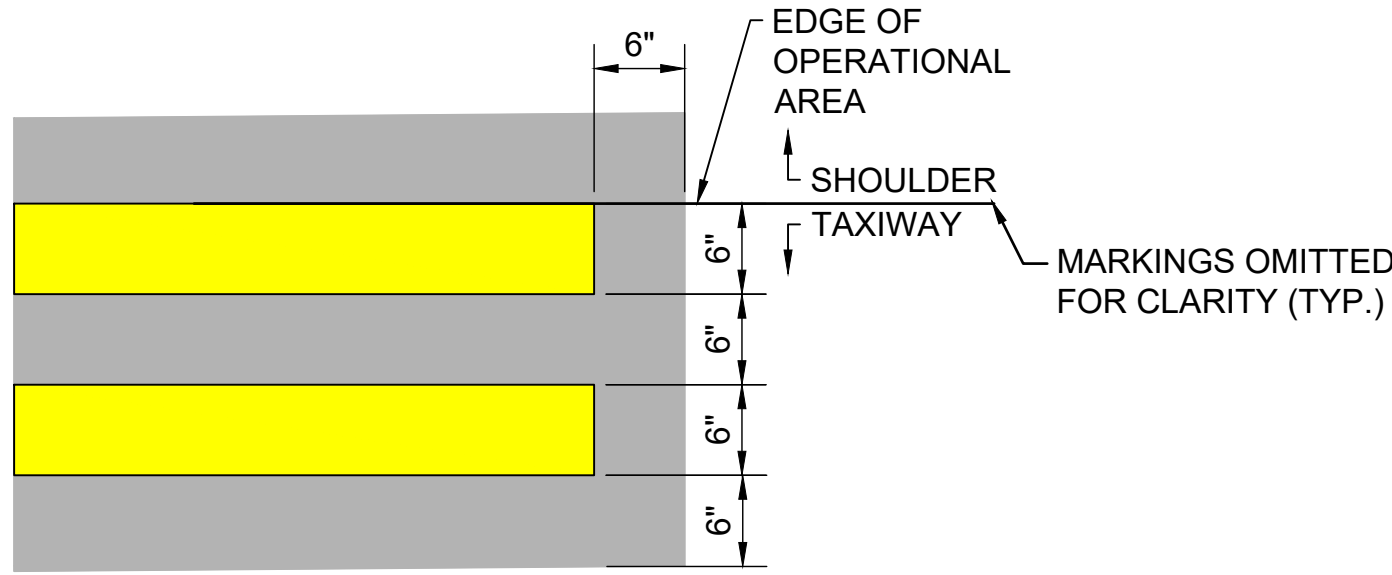


NOTE:

1. ALL YELLOW MARKINGS SHALL HAVE GLASS BEADS.
2. ALL BLACK MARKINGS SHALL NOT HAVE GLASS BEADS.
3. BLACK OUTLINE SHALL BE 6" OUTSIDE OF YELLOW MARKING.
4. DIMENSIONS DO NOT INCLUDE BLACK OUTLINE.

1
CM2.01

TAXIWAY CENTERLINE MARKINGS
SCALE: NONE

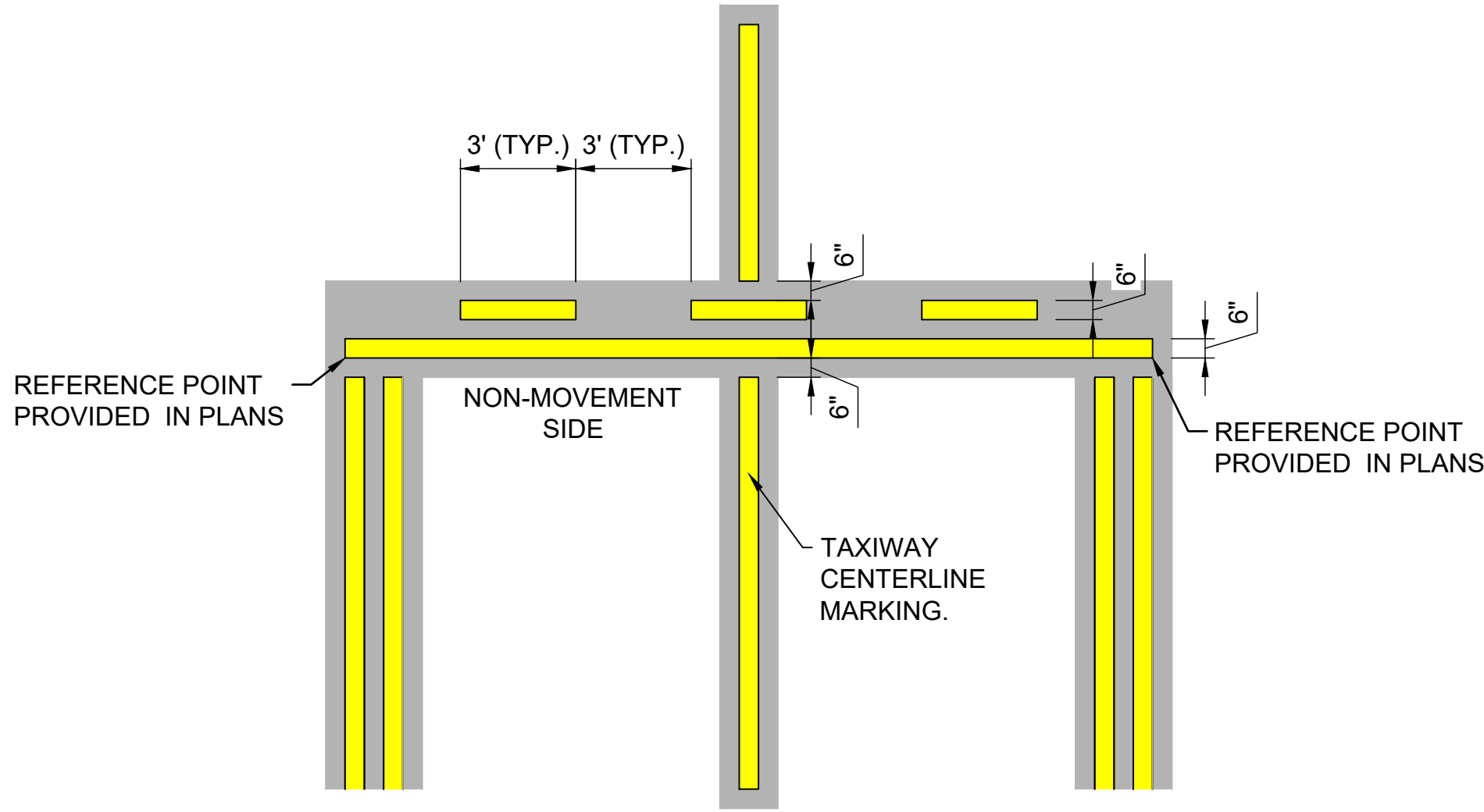


NOTE:

1. ALL YELLOW MARKINGS SHALL HAVE GLASS BEADS.
2. ALL BLACK MARKINGS SHALL HAVE NO GLASS BEADS.
3. BLACK OUTLINE SHALL BE 6" OUTSIDE OF YELLOW MARKING.
4. DIMENSIONS DO NOT INCLUDE BLACK OUTLINE.


2
CM2.01

TAXIWAY CONTINUOUS EDGE MARKINGS
SCALE: NONE



3
CM2.01

NON-MOVEMENT AREA MARKING
SCALE: NONE



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(256) 534-5512

REV.	DATE	DESCRIPTION	BY

BIRMINGHAM-SHUTTLESWORTH INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR TAXIWAY PAVEMENT REPAIR PROJECT

AIRFIELD PAVEMENT MARKING DETAILS

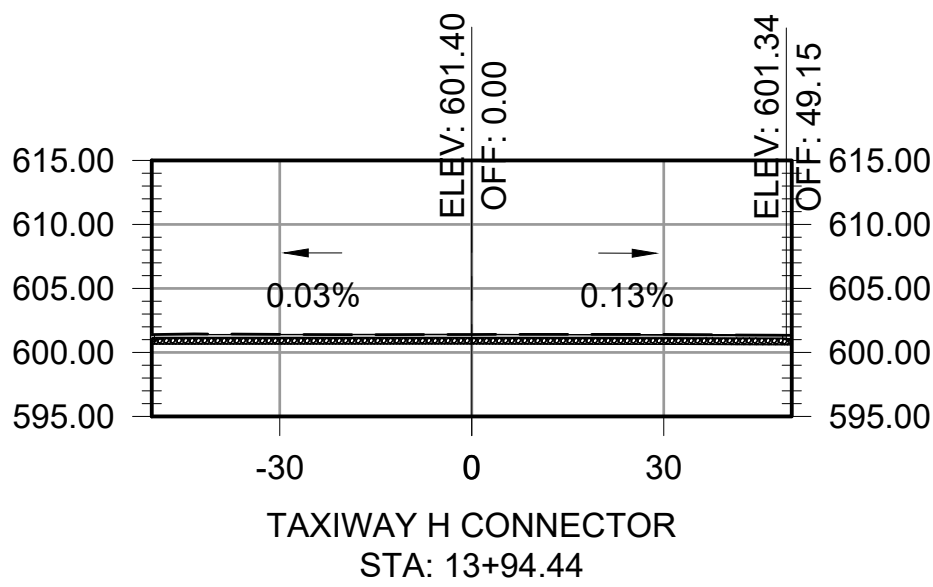
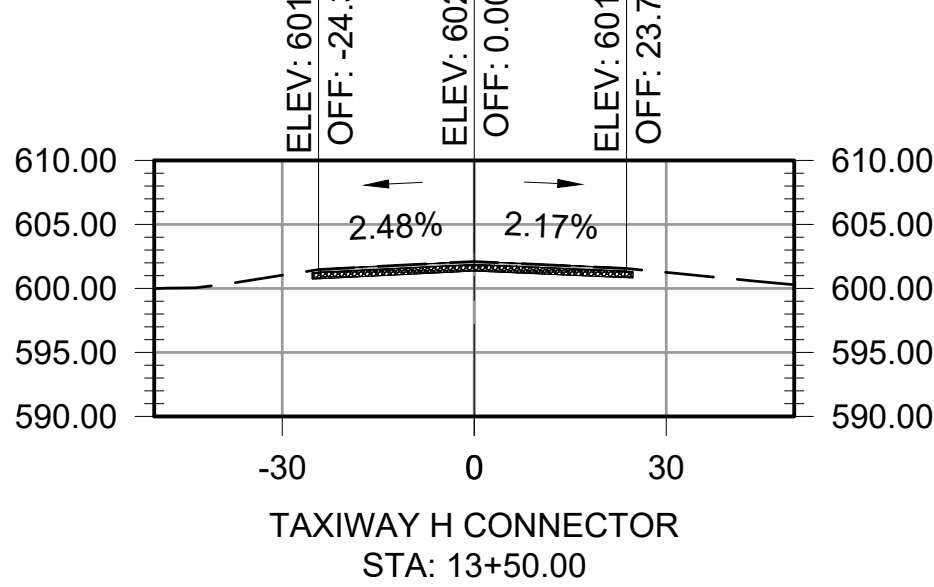
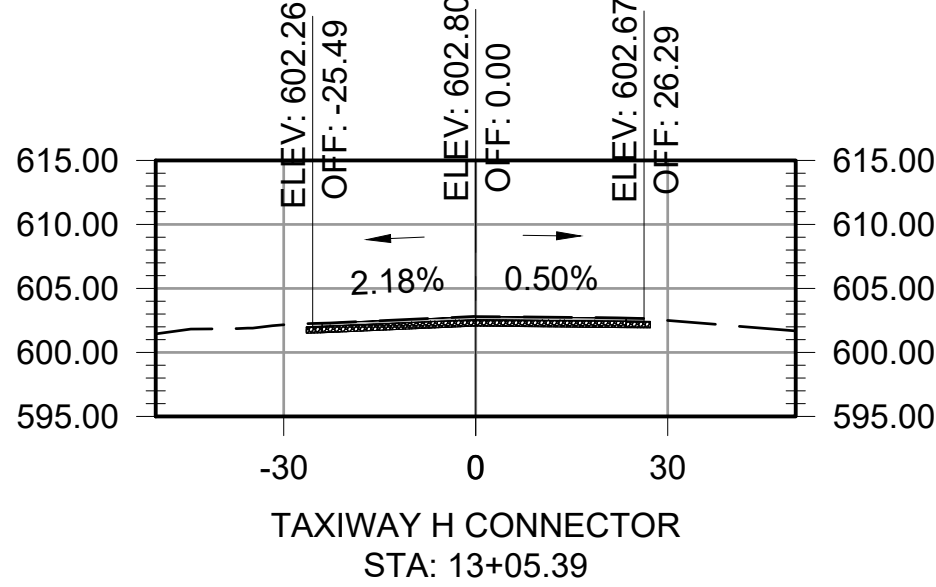
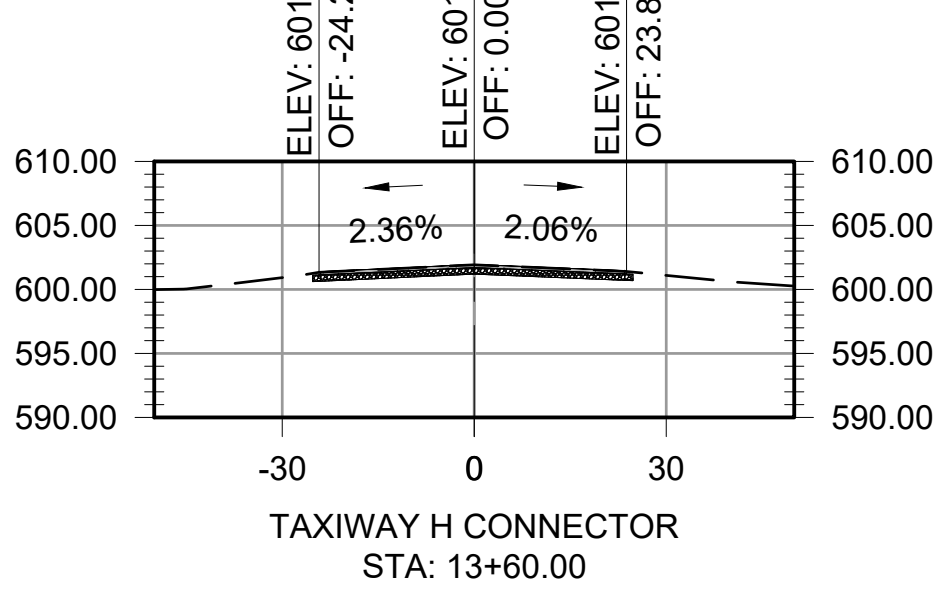
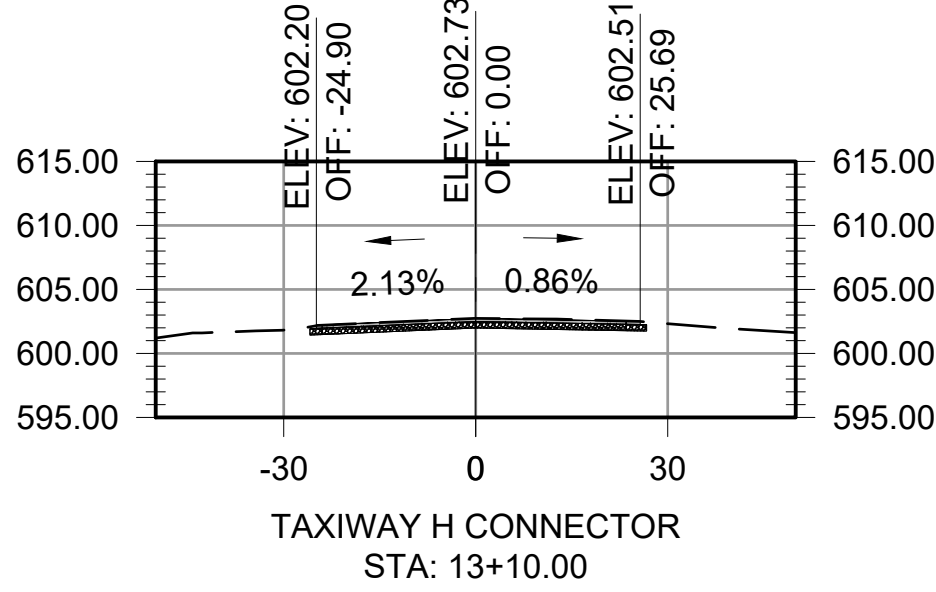
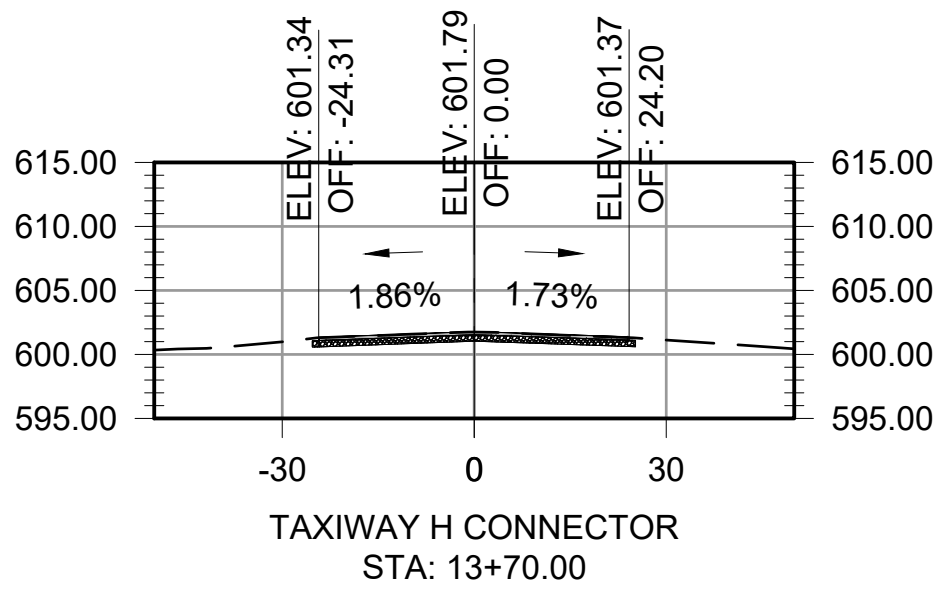
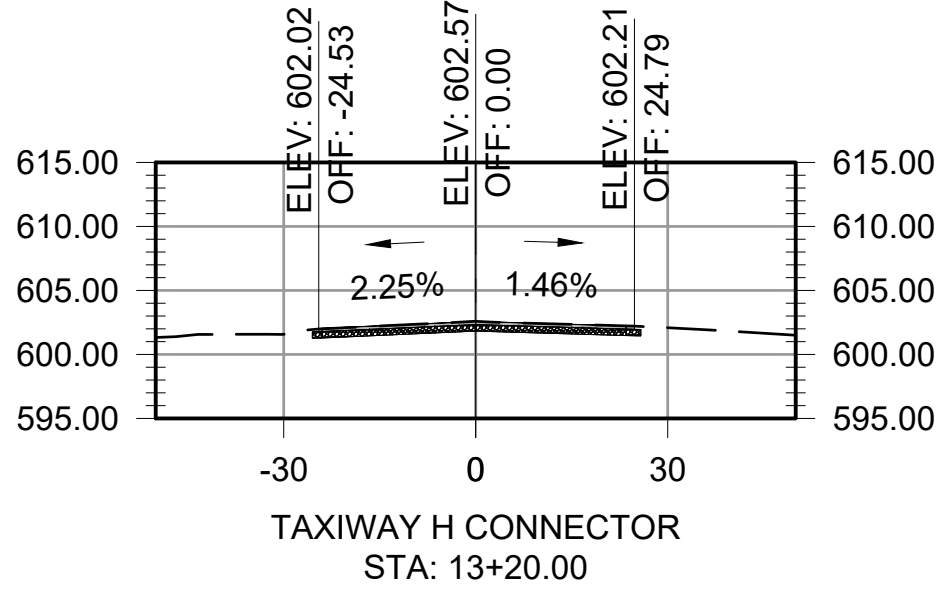
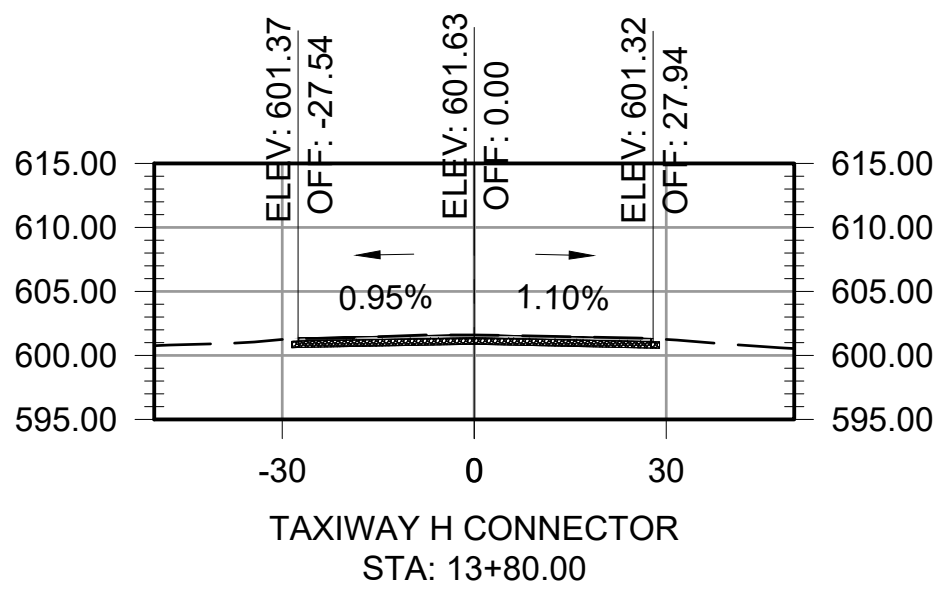
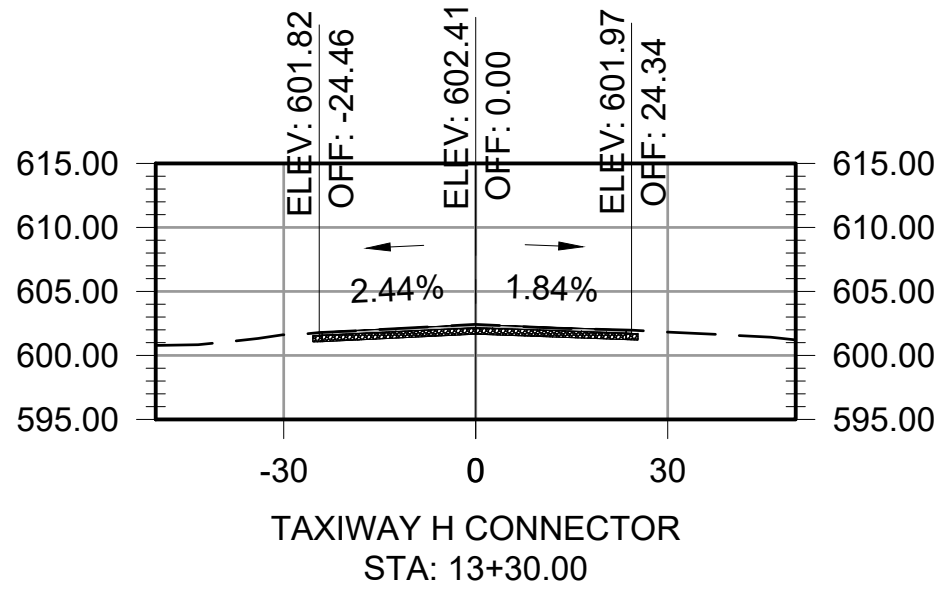
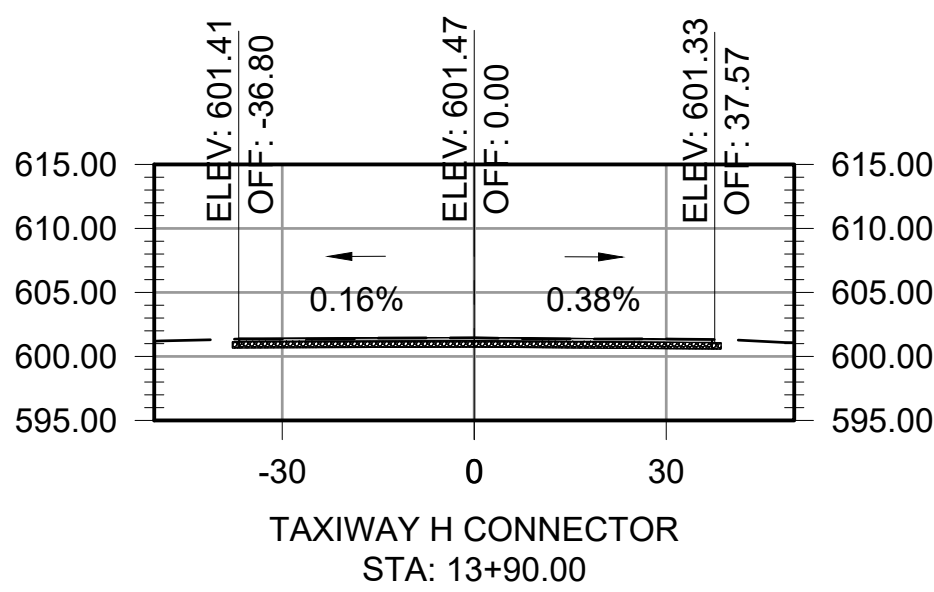
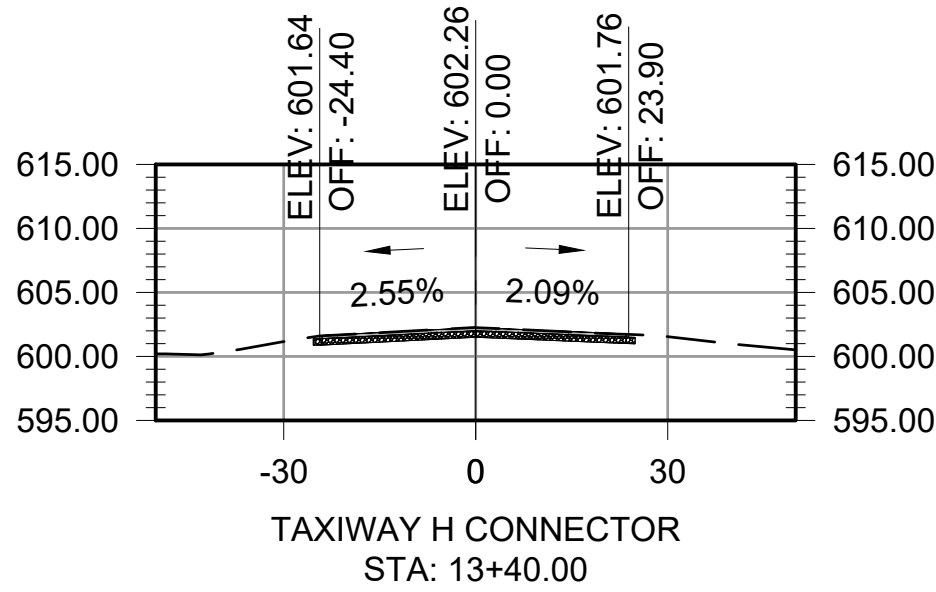
JOB NO.: 2302499
DATE: MAY 2025
DESIGNED BY: MAH
DRAWN BY: MAH


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REV.	DATE	DESCRIPTION	BY

BIRMINGHAM-SHUTTLESWORTH
INTERNATIONAL AIRPORT
BIRMINGHAM, ALABAMA

EAST ATLANTIC CONNECTOR
TAXIWAY PAVEMENT REPAIR
PROJECT

EAST ATLANTIC
CONNECTOR
TAXIWAY CROSS
SECTIONS

JOB NO.: 2302499
DATE: MAY 2025
DESIGNED BY: MAH
DRAWN BY: MAH

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XS1.01